



# *San Marco Avenue* **Design Standards for Entry Corridors**

Approved June 25, 2018



## **City of St. Augustine Commissioners**

**Mayor Nancy Shaver**

**Vice Mayor Todd Neville**

**Commissioner Leanna Freeman**

**Commissioner Roxanne Horvath**

**Commissioner Nancy Sikes-Kline**

## **City of St. Augustine Planning & Building Department**

**David Birchim, AICP**  
Director

**Amy McClure Skinner**  
Project Manager + Deputy Director

**Prepared by Marquis Latimer + Halback, Inc.**  
Approved June 25, 2018



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## Section 1. Purpose and Intent

The purpose of the standards contained in this document is to:

- a. Promote the unique characteristics of the San Marco Avenue overlay area that distinguish it from “Anywhere, USA”;
- b. To facilitate the development of vacant land and the reimagination and repurposing of underutilized properties in this area into a high-quality, multi-modal mixed-use environment;
- c. To provide standards and criteria by which proposed development in the area will be reviewed.

### 1.1. Historic Background

The San Marco Avenue Entry Corridor is a unique, neighborhood-oriented shopping, retail, and dining district that extends the pedestrian core from downtown St. Augustine. San Marco Avenue primarily developed between the 1890s and 1930s and stretches from US-1 on the north to West Castillo Drive on the south. The development boom was spurred by the introduction of a street trolley system that ran from Downtown St. Augustine to the Florida School for the Deaf and the Blind, creating a major commercial district just outside of the downtown area.

The mission of the Design Standards is to honor the history of the area and its connections to Downtown St. Augustine as well as to provide and plan for the future evolution of San Marco Avenue.

### 1.2. Neighborhood Context

Although San Marco Avenue is a commercial corridor, the roadway bisects historic neighborhoods, including Nelmar Terrace, Fullerwood, and the Abbott Tract. As the area developed, the residential character once prevalent along San Marco Avenue still remains in some areas. Numerous single-family residences dot the Avenue, converted to more contemporary business offerings while maintaining their residential scale and feel.

### 1.3. Introduction to Typologies

Traditional commercial and residential typologies define the corridor. The standards that follow herein allow for infill of these two architectural typologies, which will help bring more attention to the iconic pieces of architecture defining the main Institutional users: the Florida School for the Deaf and Blind, the Mission



Figure 1. Scenes from San Marco Avenue



Nombre de Dios Shrine, the City of St. Augustine Waterworks building, and the Mark Lance Armory (Florida National Guard).

## 1.4. Introduction to Districts

Major districts within the corridor have emerged, loosely delineated by Picolata Road (north / south).

Each of the districts contain various defining elements and structures:

### **South District (West Castillo to Picolata Road)**

The South District is home to a concentrated retail and dining area. This district, the closest to Downtown St. Augustine, exhibits the densest development, with most traditional commercial structures sitting at the sidewalk edge with little to no side separation. The district is also defined by Frame Vernacular residences (many with Victorian ornamentation), some of which are converted to business uses.

### **North District (North of Picolata Road)**

The north district is a commercial district filled with numerous local eateries and attractions.

**The standards contained herein do not address the public right-of-way, but engagement with the pedestrian sidewalks is a critical element that is addressed.**



*Figure 2. Scenes from San Marco Avenue*



## Section 2. Definitions

**Awning** - a rectangular or curved shelter extending from a building and supported entirely from the exterior wall of a building and composed of non-rigid materials except for supporting framework.

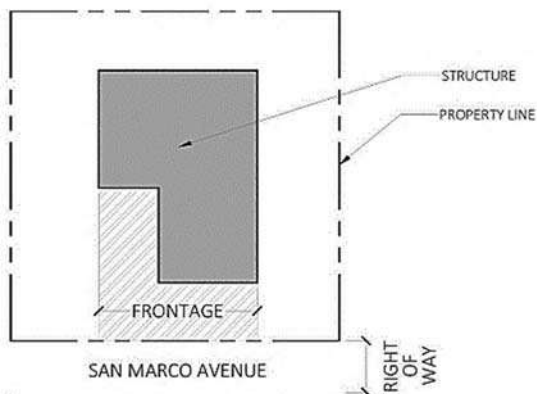


**Canopy** - a structure constructed of rigid materials including, but not limited to metal, wood, concrete, plastic or glass, which is attached to and supported by a building or by columns, poles or braces extended to the ground.



**Corridor Review Committee**- development review board as designated by the City Commission

**Frontage**- the facade of a building, specifically abutting San Marco Avenue.



**Full Cut Off** - a lighting fixture from which none of its lamp's intensity is emitted at or above a horizontal plane drawn through the bottom of the fixture and no more than ten percent of the lamp's intensity is emitted between eighty (80) degrees and that horizontal plane.



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**Land Development Regulations** - the land development and zoning codes contained in The Code of the City of St. Augustine.

**Legacy Structures** – an existing, authentic and form giving structure as identified by the city’s Historic Preservation Officer and noted in the map included herein. Existing buildings of architectural significance that have retained physical integrity and contribute to the historic character of the San Marco corridor (see map in Appendix D) based on the presence of traditional building materials and an original form/opening patterns of building as evidenced by physical inspection and sources of data such as the Florida Master Site File (FMSF).

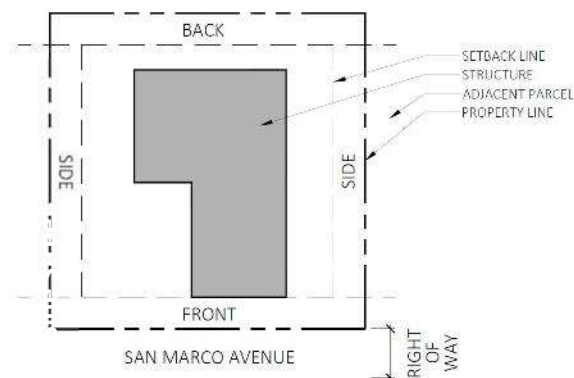
**Photometric** – a plan showing the simulated lighting coverage of a proposed development, helping designers assess if a plan will be sufficient in lighting a space while not creating a lighting disturbance to the surrounding properties

**Porte-Cochere** - a covered entrance large enough for vehicles to pass through

**Rendering** – a graphic (developed by a licensed professional) depicting a proposed structure in an existing context

**Right of Way (R.O.W.)** – publicly owned lands, typically with roadway, sidewalks, etc., which is often immediately adjacent to a property line.

**Setback** - the horizontal distance between the face of a building (excluding steps and overhangs) and the property line.



**Sign, Awning** – a sign that is painted, silk-screened, stitched on, imprinted on, or otherwise applied directly onto the fabric of the awning.







**Sign, Canopy** - a sign which is suspended from, attached to, supported from or forms a part of a canopy.



**Sign, Freestanding** - a sign supported by a sign structure secured in the ground and which is wholly independent of any building, fence, vehicle or object other than the sign structure for support. This sign type is prohibited on San Marco Avenue (see Pedestrian Pole Sign for residential typology).

**Sign, Hanging** - a sign that is placed under a canopy, awning or arcade, perpendicular to the building façade, and not intended to be seen by motorists.



**Sign, Pedestrian Pole Sign** – a sign supported by one or two poles in the ground fully independent of any building, fence, vehicle, or object, not to exceed the dimensions included in the San Marco Avenue Design Standards. This is only for the residential typology.

**Sign, Projecting** – a sign which is affixed and displayed perpendicular to any building wall or structure with a bracket



**Sign, Wall** – a sign that is attached flat to a building wall.





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**Sign, Window** – a permanent sign affixed to, or painted on either face of a window or glass door that leads to the exterior of the building. Signs suspended within three (3) feet behind the glass are also deemed window signs.







## Section 3. When the San Marco Avenue Entry Corridor Design Standards Apply

The San Marco Avenue Design Standards shall apply to new development and redevelopment on sites abutting San Marco Avenue between West Castillo Drive and the Highway US 1 Interchange and the northern portion of US-1 (see Appendix A). The following subsections specify the level of compliance required based on the development activity proposed.

The standards are in addition to the requirements of the City's comprehensive plan, zoning, and land development regulations found in the City's code of ordinances. **These standards apply to parcels fronting the San Marco Avenue corridor. The public right of way (ROW) is not included in these standards.**

While building massing and position on the site may shift due to these standards, the general building mass allowed is per the City's comprehensive plan, land development code, and ordinances. The intention of this document and all regulations set forth herein is for no net loss of buildable square footage on any site along the San Marco Avenue Entry Corridor.

- a. **New Construction.** All new development along San Marco Avenue, including new buildings or structures on vacant or already developed sites, shall comply with all the regulations contained in this document.
- b. **Substantial Expansion or Redevelopment of Existing Buildings.**
  - 1) Full compliance. An entire development shall be brought into *full* compliance with this section if the redevelopment constitutes a substantial improvement as required by city code.
  - 2) Exceptions. The following shall be the only exceptions to full code compliance for substantial expansion or redevelopment:
    - i. Building setback. Existing buildings will not be required to be moved or expanded to meet the setback requirements. However, building additions shall meet the required setback. All *new* buildings within the redevelopment site shall be required to meet the building setback provisions.
    - ii. Building frontage. Existing buildings shall not be required to meet the minimum building frontage requirement. However, new buildings and additions shall be required to comply with the frontage requirements.
    - iii. Driveways and Parking. See Section 6.6 for allowed exceptions to existing driveways and parking facilities.
  - 3) Other LDC Provisions. Compliance with parking, landscaping, screening and all other regulations stated in the city's land development regulations and not addressed in this document shall be addressed as required by the City's land development regulations.
- c. **Non-Substantial Additions or Redevelopment of Existing Buildings:** For proposed building additions or redevelopment not meeting the criteria of Subsection b.1, above, only the new additions or exterior building modifications shall comply with the regulations contained in this



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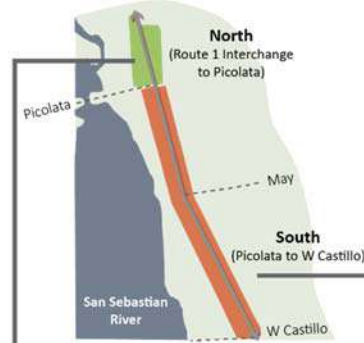
document. The remainder of the building and the site shall not be required to meet the standards.

- d. **Cumulative Improvements.** Cumulative improvement shall be addressed per City code.
- e. **Change in Use.** Changes in use (to any other permitted use in the zoning district) without any physical improvements to the site are not subject to the San Marco Avenue Design Standards.
- f. **Non-Conforming Uses:** Any changes to non-conforming uses shall be conducted per the requirements of the City's land development regulations (Chapter 28, Division 4, Nonconforming Lots, Uses and Structures).
- g. **Ancillary Uses and Structures:** Accessory uses and structures, included but not limited to parking on adjacent or nearby sites, shall be subject to the San Marco Avenue Design Standards.
- h. **Interior Changes:** Interior changes shall not be subject to the requirements of this document.
- i. **Corporate Architecture:** All national and international corporate commercial buildings shall abide by the San Marco Design Standards, as applicable by law. See Section 4.7.2.
- j. If a provision of these Standards is or becomes illegal, invalid or unenforceable, that shall not affect the validity or enforceability of any other provision of these Standards.



**1**

**Identify Your District**



**Typology Pathway**  
San Marco Avenue Entry Corridor

**2**

**Determine Typology**

Select

**Commercial Typology**  
Includes commercial, auto service, mid century lodging, and suburban commercial

**SETBACKS**  
0' to 10' front

**LANDSCAPE**  
Street trees to include Live Oaks and Sabal Palms  
Planters to reflect style of the structure

**PARKING REQUIREMENTS**  
Per Code

**Residential Typology**

**SETBACKS**  
15' min. front

**LANDSCAPE**  
Front lawn and plantings evocative of the residential typology  
Landscaped plazas acceptable

**PARKING REQUIREMENTS**  
Per Code with 25% reduction in parking stall requirements

**Institutional Typology**

**SETBACKS**  
10' min. front

**LANDSCAPE**  
Per Code

**3**

Select

**Commercial NORTH**

Traditional Commercial  
Modern/Contemporary

**Commercial SOUTH**

Traditional Commercial

**Residential SOUTH**

Florida Vernacular

**Institutional**

Dependent on existing insitution



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## Section 4. San Marco Avenue Design Standard Application Review Process

The zoning and building codes regulate land uses, construction and signs. Developers must obtain a permit from the planning and building department prior to commencing work or installing signs. Projects involving land disturbance may require investigation by the city archaeologist prior to work.

### 4.1. Application Process

Application forms and instructions may be obtained from the planning and building department. Applications must be signed by the property owner. An application fee is required. Supporting documentation, such as plans, elevations, sketches, photographs, samples of materials and specifications shall be submitted with the application.

### 4.2. Plan Review

Staff from the planning and building department shall be responsible for reviewing development plans to determine compliance with the San Marco Avenue Design Standards. The city may decide to contract with a design consultant to perform architectural reviews and determine consistency with the provisions of this document.

### 4.3. Corridor Review Committee

#### 4.3.1. Activities Requiring Review.

- a. Modification of standards between 10 and 30% of a dimensional requirement of this document and non-dimensional modifications (color, landscape, etc.). Changes over 30% are not permissible;
- b. Development sites that exceed one third (1/3) acre in size;
- c. Developments that exceed 2,500 square feet in gross floor area;
- d. New construction that includes parking structures;
- e. Use of neon or color LED;
- f. Colors not pre-approved.
- g. Appeals.

#### 4.3.2. Review Criteria.

The CRC shall use the following criteria (in addition to the standards contained herein) when reviewing an application:

- a. The proposed project meets the intent of the San Marco Avenue Design Standards as defined in Section 1;
- b. Approval of the proposal will not set an unintended precedent; (note that no review decision creates assumption of precedent)



- c. Approval of the request will not be detrimental to the physical characteristics of the neighboring sites or the overlay district as a whole, with respect to the physical characteristics prescribed within the authority of this document.

#### 4.4. Effect of Approval.

If the plans are approved by the planning and building department staff (or designated entity), the applicant may proceed to apply for a building permit. Project plans will be considered approved with the issuance of a building permit. If the proposed use requires a zoning exception or variance, the applicant shall submit to the Planning and Zoning Board for additional approval(s).

#### 4.5. Appeals

Staff determinations may be appealed to the CRC. CRC determinations may be appealed to the City Commission.

Applicant appeals must be made in writing within two weeks after a staff denial, or thirty days after a CRC denial. Appeal should state item number, address, action and justification for reversal or modification. Affected parties, other than applicant, shall have 30 days for both staff and CRC decisions to be appealed.

#### 4.6. Submittal Requirements.

For signage and painting, submittals shall be provided by a professional and/or the owner. For building modifications requiring building permits, all plans shall be prepared by appropriate registered professionals including but not limited to; architects, landscape architects and engineers. Plans submitted must be neat, legible, drawn to scale and dimensioned. Failure to submit complete plans will result in the plans being returned as incomplete. The plans shall contain the following at a minimum:

- A complete site plan of the property to be developed, showing the location of all structures and buildings, required yards, required parking, surface drive areas, loading spaces, stacking spaces, landscaping areas (both perimeter and interior), the location of all trees three (3) inches or larger dbh, by species and dbh (both to be removed and to be retained), dumpsters, exterior mechanical equipment, storm drainage retention areas, etc. (scale preferred: 1 inch = 20 feet).
- Landscape planting plan at a minimum scale of 1 inch=20 feet including hardscape, plant materials, sizes and location.
- A list of any requested modifications of standards.
- A clear and detailed description of all proposed construction.
- Elevations of all proposed construction (scale preferred: 1/8 inches = 1 foot).
- Wall sections (scale preferred: 3/4 inches = 1 foot).
- Northerly and easterly sections through the site focusing on the proposed building(s), existing building(s), and sidewalks. All abutting roadways should be shown. Preferred scale: 1/8 inches = 1 foot.
- Images presenting the structure's mass and form in relation to its surrounding structures must be included for all new construction. A minimum of two drawings, must include parcel surroundings plus cross street adjacencies. (Tools such as SketchUp, which is free and readily available, is recommended).
- Paint colors and locations (including paint chips or samples).
- Photographs of the existing structure (pre-construction) and all adjacent structures.





- Details indicating the following (scale preferred: 1-1/2 inches = 1 foot) windows, shutters and shutter hardware
- Doors, hinges and hardware
- Light fixtures and photometric plan for site lighting over 8' in mounting height
- Gutters and downspouts
- Exterior surfaces, materials and textures
- Chimneys
- Roofing materials and design
- Air conditioning equipment and above ground fuel tanks
- Electric meters and service risers
- Satellite dish antennas or other similar equipment, location and dimensions
- Exterior porches, landings, stairs, ramps, railings and banisters
- Fences and walls (height, materials and colors)
- Walks and drive surface materials
- Patios and decks
- Other miscellaneous ornamentation
- Renderings, optional (mandatory for new construction)

#### 4.7. Modification of Standards

Due to the individual unique characteristics or circumstances of any given development along San Marco Avenue, flexibility in the application of design standards may be warranted in certain situations. Modifications from the standards may be requested by an applicant as part of the development review process. If an applicant requests multiple modifications, each modification shall be evaluated independently.

##### 4.7.1. Application.

All requests for modifications shall be noted on the site plan. The request shall be approved or denied during development plan review and, if granted, shall be noted on the final development plan.

##### 4.7.2. Administrative Modifications.

The Planning and Building Department Director or designee shall have the authority to grant limited modifications, as set forth below, where it is determined that the proposed development meets the intent of the zoning district, the requested modification is the minimum necessary to allow reasonable development of the site and the requested modification is not injurious to the public health, safety and welfare. Administrative modifications are limited to ten (10) percent of the dimensional requirements established in this document. An applicant that meets the statutory criteria of Section 553.79(20)(a), Florida Statutes, must clearly identify in the application materials the specific elements of the proposed project that apply to the statute. Once identified, these statutory design elements will be approved administratively.

##### 4.7.3. CRC Modifications.

The CRC shall review the following modification of standards:



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- Modifications of more than ten (10) percent but no more than thirty (30) percent of a dimensional requirement,
- Modifications of ten (10) percent or less previously denied or referred to the Corridor Review Committee by staff, and
- Modifications to the non-dimensional requirements contained in this document.

#### 4.7.4. Prohibited Modifications.

No modification shall be granted under this section for the following:

- a. Use of property as defined in Land Development Regulations.
- b. Maximum levels of density or intensity allowed in comprehensive plan.
- c. Encroachments into the public right-of-way.
- d. A reduction of a setback to less than 5 feet adjacent to a single family residential district.

#### 4.7.5. Review Criteria.

A modification of standards may be granted by the approving authority if it finds that strict application of the standards is not warranted and that granting a modification will fulfill the intent of the standards. The approving authority shall apply all the following criteria, when applicable, to determine if the applicant has justified a request for a modification:

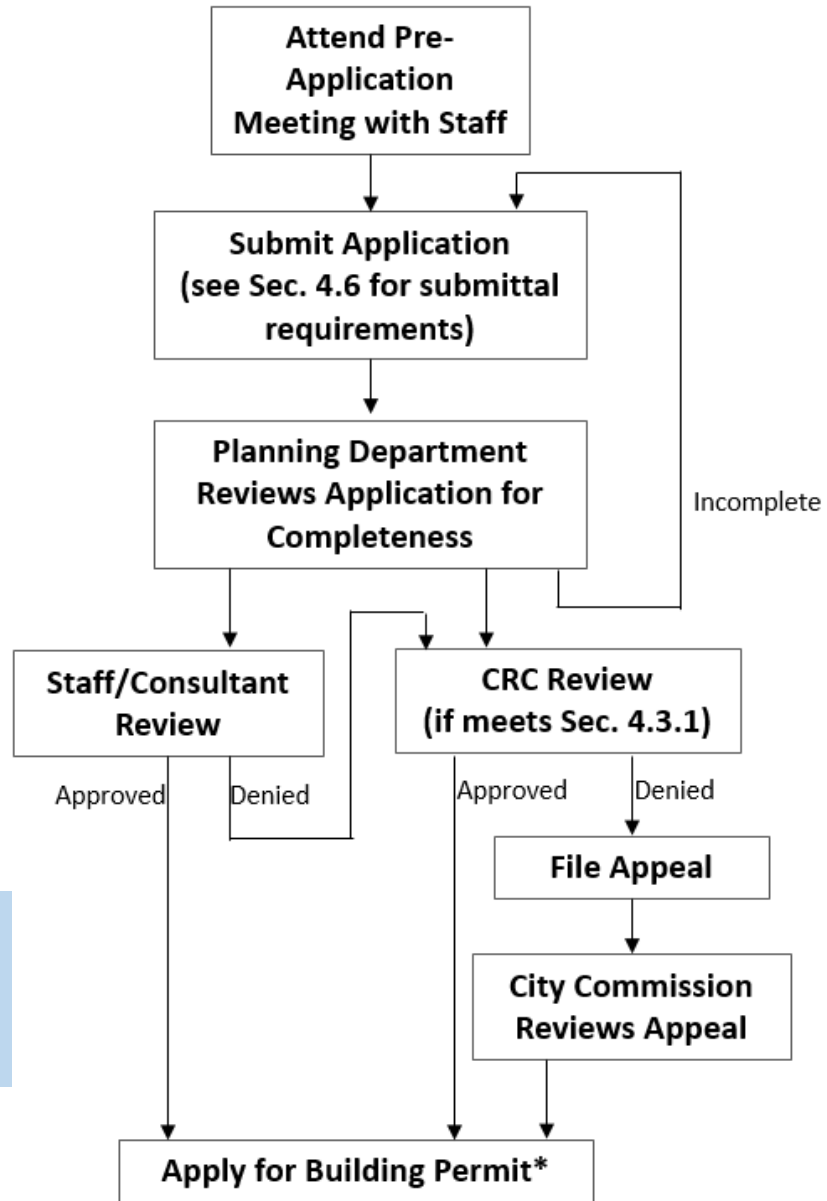
- a. The request is within the parameters listed above.
- b. The request is consistent with the Comprehensive Plan and generally consistent with the purpose of the city's Land Development Regulations and this document.
- c. The proposed modification will not have a material negative impact on adjacent uses, or the applicant proposes to mitigate the negative impact to be created by the modification.
- d. Compliance with the requirement is technically infeasible based on site conditions.

#### 4.7.6. Additional Requirements.

- a. The burden of presenting evidence sufficient to satisfy the applicable criteria remains with the applicant seeking the modification.
- b. The applicant may propose conditions to ensure that the use of the property to which the modification applies will be reasonably compatible with the surrounding properties, including visual screening.
- c. A granted modification and any conditions attached to it shall be entered on the face of or attached to the development order or permit and development plan.
- d. A proposed change to an approved modification shall be added to the approved modification and considered in the aggregate. The total modification (approved modification plus proposed change) shall determine the approving authority per subsections 4.7.2 and 4.7.3, above.



Figure 3. Review Flow Chart



**NOTE:** If trees are to be removed, approval must be granted by the Planning and Zoning Board before proceeding to the Corridor Review Committee.

\* Not including other reviews such as zoning exception or variance. If those are required, they occur after the CRC review.



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## Section 5. Development Typologies

The San Marco Avenue entry corridor has two distinct characters. The area south of Picolata Road presents a mix of buildings with a historic character, some originally designed for residential use while others were originally intended for commercial use. The area north of Picolata Road introduces industrial/utilitarian buildings and more modern structures. Both areas contain institutional buildings (e.g. the historic Water Works building, the National Guard Armory, the Florida School for the Deaf and the Blind). Future infill and redevelopment is required to perpetuate the characteristics of each area by adopting and/or reinforcing one of the following building typologies.



*Current and encouraged site/building typology along San Marco Avenue south of Picolata Road*

### 5.1. Residential Typology

Even though the name and appearance of this typology refers to a residential use, any of the permitted uses in the zoning district may utilize this design if located in the area **south of Picolata Road**. Where a building exists that is considered a legacy building, redevelopment shall maintain the character features of the building and site arrangement.

The reason for allowing and encouraging the use of this development typology is to preserve the current character of the south area of the San Marco Avenue corridor, which mixes residential and commercial development types in a single block.



Typical characteristics of the current Residential Typology include two story to two and one-half story buildings setback from the street at least ten feet, allowing room for landscaping and a front porch. The main entrance faces the street, and windows are traditionally vertical in orientation and include top and bottom panes (single/double hung). Building materials typically include wood siding and in a few instances, stucco. Roof design is sloped in gable or hip form, and usually finished with shingles or metal.

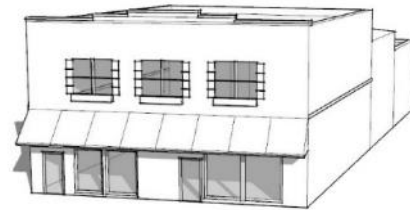
While replicas of buildings are not allowed, new development proposing to use this typology, additions, and modifications will be required to respect the main features of this type of building. New developments and conversions using this typology will be eligible for a waiver of parking space requirements with a reduction of overall parking by 25% (except for handicap spaces), and will be eligible for additional signage (see Section 8). This reduction to the required parking will aid in maintaining a 'front yard' with an adequate setback characteristic of the residential typology.



*Single family homes converted to commercial use (expansive setbacks, two stories, porches)*

## 5.2. Commercial Typology

This typology refers to the commercial buildings of the early twentieth century found along San Marco Avenue. This typology may be used for new development, additions and modifications anywhere along the corridor and for any use permitted in the zoning district. Where a building exists that is considered a legacy building, redevelopment shall maintain the character features of the building and site arrangement.



The main characteristics of this typology include one to two story buildings with retail uses occupying the ground floor and residential or commercial uses above. The ground floor usually includes storefront windows and glass doors and a canopy or awning. The buildings are typically placed close to the public sidewalk. Building materials are typically masonry (brick, stucco). Roofs are typically flat and include a parapet. Developments in the south district (south of Picolata Road) using this typology will be required to meet the architectural standards based on the traditional design of such buildings. Developments in the north district (north of Picolata Road) may utilize more modern architectural styles, but will still be required to meet the general building design standards detailed later in this document.



*Buildings originally designed for commercial use (building up to the sidewalk, one or two stories)*

With the advent of the automobile, development along the San Marco Avenue corridor started to deviate from the Residential and the Commercial typologies mentioned above and started introducing auto-oriented uses. Auto-oriented site/building designs are not allowed.

Minor additions and modifications will need to be consistent with the existing building's typology, but substantial improvements (as defined in Section 3) will require the conversion of the existing building typology into the Commercial Typology described above.





### 5.3. Institutional Typology

Public, civic and institutional buildings are intended to stand out and to serve as landmarks in the community. For that reason, they typically feature a different building type and style than their neighboring buildings and it is important to establish a different set of development and design parameters for this type of facility. The placement on the site, for instance, must be able to accommodate the gathering of large groups of people. This typology may only be used for public, civic and institutional buildings.=



*Public, civic and institutional buildings are intended to stand out (unique architectural design, setbacks that accommodate public gathering and respond to individual needs of use). Specific typology styles for existing institutions are defined below. These can only be used by institutional use properties.*

<b>Institution</b>	<b>Architectural Style*</b>
<b>Mission Nombre de Dios</b>	Contemporary/Modern
<b>Waterworks Building</b>	Masonry Vernacular
<b>National Guard Armory</b>	Mission Revival
<b>Florida School for the Deaf and the Blind</b>	Mediterranean Revival

\*Traditional commercial may also be used by institutional users.



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## Section 6. Site and Building Design

The rhythm and pattern of buildings, combined with their relationship to the street expressed through setbacks and architecture, define the character of the public space. The location of a building on a site is critical to creating a vital and coherent public realm that is both welcoming and functional and shall be determined by selecting one of the development typologies described in the previous section and meeting the following site and building design standards.

### 6.1. Building Placement

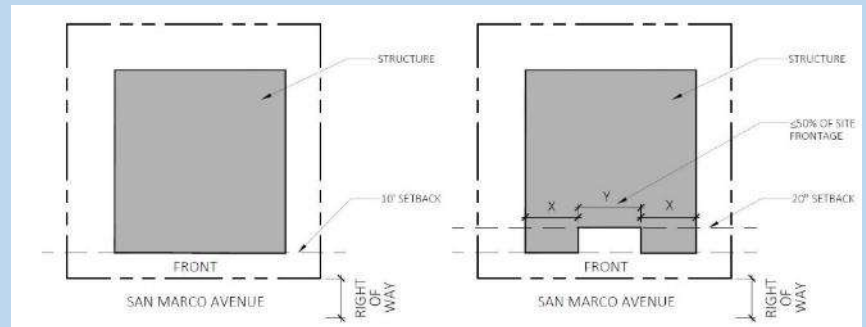
The setback standards prescribed in this section are intended to create a pedestrian-friendly public realm with buildings located close to the street (at varying degrees). The public realm typically includes all the elements within the right-of-way (sidewalks, travel lanes, on-street parking, landscape strips), but it also includes activities that occur within the front yard setbacks, which in a commercial area are considered to be the transition area between the public and the private spaces and needs to be inviting for the commercial corridor to be successful. That transitional zone in the public realm may also include expanded sidewalks, outdoor cafés, bicycle parking, art/sculptures, and outdoor retail spaces, among others. To encourage the creation of a successful public realm, the following building placement standards shall be met.

*Table 1. Building Setbacks*

	Residential Type	Commercial Type	Institutional Type
<b>Front Yard Setback</b>	15 ft. min., 30' max.	0 ft. min., 10 ft. max.	10 ft. min. (no max.)
<b>Street Side Yard Setback</b>	5 ft. min.	0 ft. min., 10 ft. max.	10 ft. min.
<b>Interior Side Yard Setback</b>	5 ft. min.	0 ft. min.	10 ft. min.
<b>Rear Yard Setback</b>	5 ft. min.	5 ft. min.	10 ft. min.
<b>Setback adjacent to single family zoning</b>	15 ft.	15 ft.	15 ft.

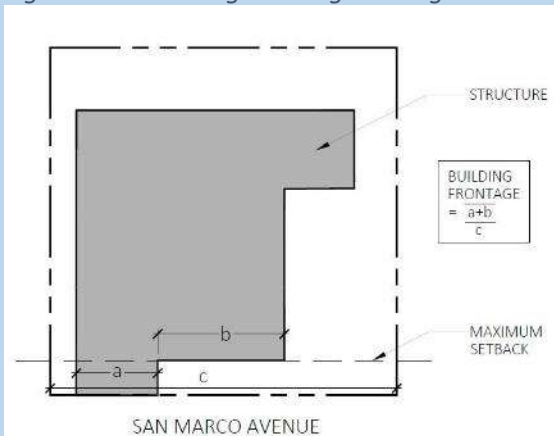
- a. New Commercial Type developments adjacent to a development already using a 0-foot side yard setback shall provide a side yard setback of 0 feet.
- b. There shall be no parking or drop off areas in the front yard of any of the commercial or residential typologies allowed. Institutional buildings are exempt from this regulation.
- c. The maximum front setback for the Commercial Typology may be expanded to 20 feet for a maximum of 50% of the building frontage (as defined in 6.2 a) for use as a public courtyard, landscape, or other non-parking purposes. (see Figure 4).

Figure 4. Expanded Front Yard Setback for Courtyard



- d. The maximum front and street side building setbacks (if side street setbacks exist) shall only apply to the portion of the building meeting the building frontage requirement (see Figure 5). This provision will allow for L-shape buildings or step back setbacks, where a portion of the building is recessed more than what the setback allows.

Figure 5. Measuring Building Frontage



Only the “a” and “b” facades are required to meet the maximum building setback, assuming that  $a + b$  meet the frontage requirement of Section 6.1 (Building Frontage).

- e. The front and side street-setbacks may be used to expand the public sidewalk, for outdoor dining, or for landscaping (see Section 7).
- f. Balconies, bay windows, awnings, canopies, eaves and roof overhangs used in conjunction with the Commercial Typology may encroach into the setback zones, but not into the public right-of-way.
- g. Porches in the Residential Typology are allowed to encroach into the minimum front yard setback up to eight (8) feet.
- h. New developments or additions abutting or from a single-family residential zoning district shall provide a fifteen (15) foot setback along the portion of the site abutting or from the single family zoned property. Porches, however, can encroach upon this setback up to eight (8) feet. Landscaped buffers shall be provided as required in Section 7 of this document.
- i. Primary building facades facing a right-of-way shall be built parallel to the property line along that street (see Figure 6).



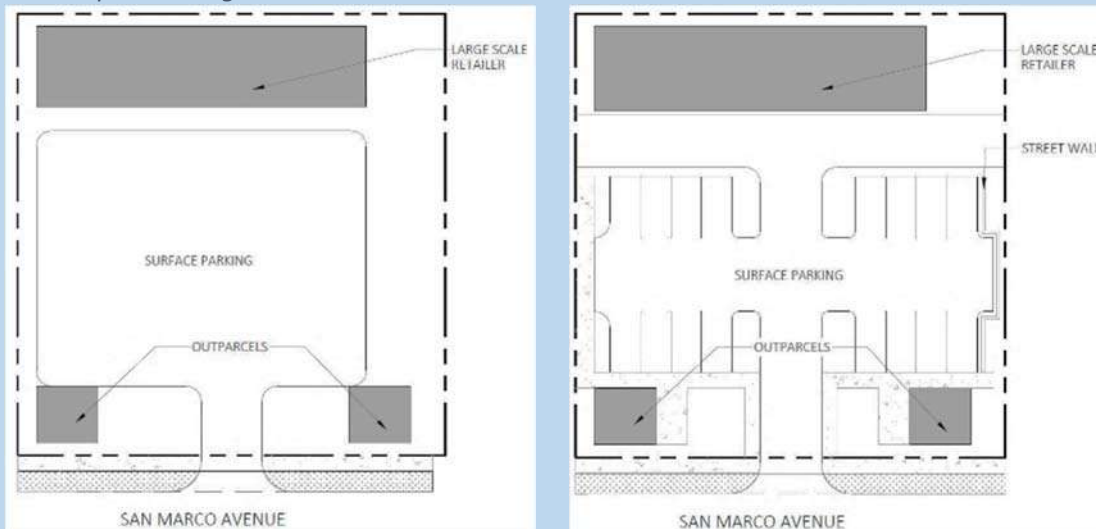
Figure 6. Building and Street Alignment



- j. There are very few large sites along San Marco Avenue. Most of the larger sites are located in the north area, where San Marco Avenue merges into US 1. In the case of large sites where there is adequate room to place buildings in the rear of the site (away from San Marco Avenue), the placement of such buildings in the rear is permitted as long as one or more buildings are built in front meeting the front yard setback requirements of the corridor (see Figure 7 & 8 for an acceptable design alternative).

The building(s) along San Marco Avenue shall be built first. Access drives shall be incorporated into the site layout to create connectivity to other sites and streets, and such drives shall be designed to resemble streets (pavement, sidewalks, markings and curbs). The main access drive to the site shall be centered on the anchor building (if allowed by driveway permits) and shall be lined with buildings or sidewalks and urban landscaping.

Figure 7. Multiple Buildings on a Site



*Prohibited*

*Recommended*

- k. Sites with frontage on San Marco Avenue and US 1 shall give priority to the development standards applicable to the San Marco Avenue corridor. The setback along US 1 must also be



deemed to be a “front” and shall meet the front yard setback of the applicable zoning district (see Figure 8).

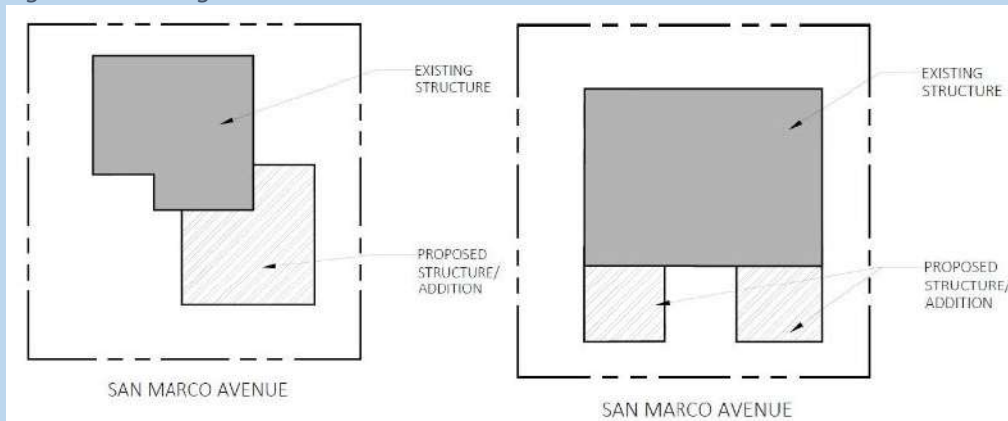
*Figure 8. Multiple Buildings on a Site*



*The aerial photograph shows a site with street frontage on four sides. The development lines up the streets (commercial along one street, multifamily along two, and live-work units on the fourth side) and parking is located interior to the block/development.*

- I. Existing buildings on a site subject to redevelopment will not be required to meet the setback standards listed above. However, if technically feasible and not detrimental to the architecture of the existing building, any additions to the building shall serve to bring the building closer into compliance (see Figure 9).

*Figure 9. Building Additions*



*Examples of building additions bringing the building closer to the street*

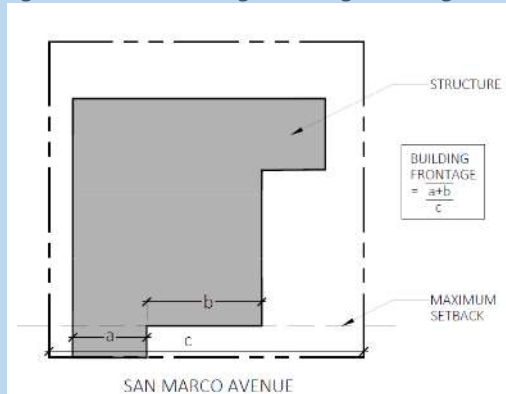
## 6.2. Building Frontage

The purpose of the following building frontage requirements is to ensure façade continuity along San Marco Avenue and to avoid large voids between buildings. The building frontage standards are stated as a proportion of the building width within the required building setback relative to the width of the



development site measured at the site frontage line. Portions of the building façade outside the required front/street building setback do not count as building frontage (see Figure 10).

Figure 10. Measuring Building Frontage



*Only “a” and “b” are counted toward the minimum building frontage requirement. The other two frontages don’t count as they are located behind the maximum front yard setback. The example at left shows 60% frontage within the maximum setback zone.*

- a. The building frontage (a+b) shall be a minimum of 70% of the site frontage (C).
- b. For redevelopment only, mid-century lodging developments are subject to a minimum 25% building frontage requirement (a+b) in order to allow the middle courtyard to remain. The courtyard, however, shall be screened with a street wall (see Section 7).
- c. Sites with frontages on multiple streets shall be required to meet the minimum primary frontage along San Marco Avenue.

### 6.3. Building Height

- a. The maximum building height for all building typologies, shall be thirty-five (35) feet. Within the first 20’, measured from the front property line, the maximum height shall be 25’ measured from base flood elevation. This portion may be utilized for a roof top deck, but any awnings or coverings shall be a minimum of 10’ from the front façade. Note that a rooftop deck shall not be placed above 27’ anywhere on the building, and an 8’ minimum solid sound barrier shall be placed on the rear face to block sound from moving toward adjacent residential areas.
- b. There shall be a minimum height of fifteen (15) feet for the Commercial Typology buildings. This height will allow the provision of adequate size windows, awnings or canopies, signage and a decorative parapet to hide roof-top mechanical equipment. See Figure 11.
- c. Residential Type buildings shall not exceed two and one half (2½) stories.





Figure 11. Minimum Building Height



*Prohibited*



*Permitted*

- d. Unoccupied accessory towers, spires and cupolas may exceed the maximum height by a maximum of fifteen (15) feet. These features shall be in character with the architecture of the building and not designed solely as sign features or attention-getting purposes.
- e. Commercial Type buildings shall have an interior first floor vertical clearance of at least twelve (12) feet to allow adequate exterior street level scale and adequate room for overhead retail utilities in ground floor spaces.

## 6.4. Building Design

The San Marco Avenue area is known for a diversity of architecture and building typology. Most buildings currently feature historic styles such as Frame and Masonry Vernacular, Traditional Commercial, and a few Georgian Revival, Modern, Mission Revival and Bungalow. There is a stronger historic character in the south portion of the corridor, serving as an extension of the historic downtown architecture. Protecting the diverse architectural character of the area is a goal of these design standards.

Building design shall be addressed based on the development typology identified in Section 5. Specific architectural styles are not required, but new development shall respect the scale and orientation of the existing development along the corridor.

### 6.4.1. Architecture

While the Spanish Colonial architectural styles are typically associated with the City of St. Augustine, these styles shall not be replicated for new construction or redevelopment along the San Marco Avenue corridor. New construction and renovations should focus on reinforcing the predominant styles currently found in the district. Frame Vernacular for the Residential Typology, and Traditional Commercial for the Commercial Typology. Contemporary architectural design, not to be confused with prefabricated/mass produced architecture, will also be permitted in conjunction with the Commercial Typology, as long as the design respects the scale and character of the preferred styles. Existing Institutional developments shall use their existing architectural style or traditional commercial on any future additions and modifications on the site. Appendix 2 lists the elements characteristic of the styles found in the corridor, which should be preserved or used as inspiration when designing new buildings or renovations. The following section contains additional building design standards that are applicable to all development regardless of the building typology or architectural style selected.



**a. Residential Typology:**

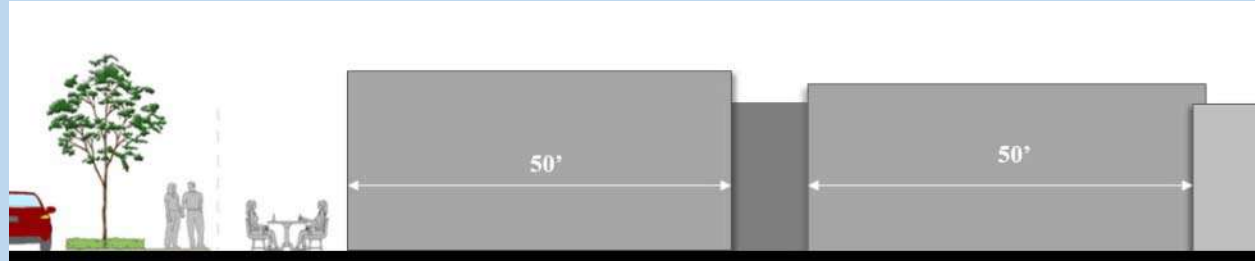
1. New buildings using the Residential Typology should utilize the Frame (Florida) Vernacular architectural style (see Appendix B).
2. All structures on a site shall be of the same architectural character (Florida vernacular architecture found throughout St. Augustine).
3. Architectural details and materials shall remain consistent on all elevations but may be simplified on the interior side and rear facades.
4. Building volumes, regardless of the use of the site, shall have a residential scale. Therefore, buildings using this design shall not exceed a width of forty-five (45) feet.
5. Each main building shall have a full or partial width front porch (open or enclosed).
6. Porch depth shall be no less than six (6) feet and no greater than twelve (12) feet.
7. Porches shall extend for at least forty (40) percent of the façade width.
8. The main roof shall be sloped (side gable or hip). The porch roof may be pent, shed, gable or incised.
9. Roof dormers are encouraged and shall be no wider than the width of the window plus corner casing.
10. Dormer windows must be consistent with all the other windows. Painted glass shall not be allowed.
11. The street side façade shall be undulated – recesses or projections will be required every thirty (30) horizontal feet of the façade.
12. Windows shall have a vertical orientation and may be single or double hung. Paned windows are encouraged but not required.
13. Frosted or patterned glass is not permitted on windows facing the street. Stained glass, however, is permitted.
14. Shutters, if used, shall be one half the width of the window and shall be (or appear to be) operable.

**b. Commercial Typology:**

Buildings using the Commercial Typology shall adopt the Traditional Commercial architectural style, described in Appendix B, if located south of Picolata Road. Developments north of Picolata Road are allowed to use the Traditional Commercial or a modern/contemporary style. Regardless of the architectural style, all development using the Commercial Typology must meet the following standards:

1. Large building volumes shall be divided to appear as smaller volumes grouped together. Volume breaks may be achieved by building wall projections and recesses, and varying heights and roof lines. Therefore, building facades shall not exceed fifty (50) feet along a street frontage without providing a substantial volume break such as a recess, or an architecturally prominent public entrance (Figure 12). The recesses and projections shall have a minimum depth of two (2) feet.

Figure 12. Building Massing Example



2. Façades shall not exceed twenty (20) horizontal feet and ten (10) vertical feet without including at least one (1) of the following elements (see Figure 13):
  - (a) A window or door
  - (b) Awning, canopy or marquee.
  - (c) An offset, column, reveal, void, projecting rib, band, cornice, or similar element with a minimum depth of six (6) inches.
  - (d) Arcade, gallery or stoop.
  - (e) Complementary changes in materials or texture.
3. Architectural treatments on the façade, such as parapets, cornices or expression lines, shall be continued around the sides of the building.

**c. Existing Buildings/Renovations and Additions**

Existing buildings that are being renovated or expanded shall meet the following standards:

1. Buildings that already feature a particular style or present some of the characteristics of a style shall be carefully restored to preserve such features. Otherwise, they should be redesigned to improve compatibility with the context of San Marco Avenue. Where a building exists that is considered a legacy building, redevelopment shall maintain the character features of the building and site arrangement. Primary features such as window shapes and sizes, opening patterns and entry doors, roof shapes and material pattern, siding patterns and finishes, porch and stair elements, foundations, and storefronts may be repaired and altered to the extent that the overall character of these elements is maintained.
2. Additions shall be designed so that there is the least possible loss or obstruction of character-defining features. The dominant features and facades of the existing building shall continue to be the prominent visual feature of the streetscape such that the new addition will not block or dwarf the existing building.
3. For rehabilitations of existing buildings, historic features used for architectural detail shall be preserved, restored, or reconstructed unless technically or economically infeasible, unless the removal is approved by the CRC. Removal will only be permissible if deemed necessary by the CRC. The Applicant shall note the historic and architecturally significant elements on the building elevations submitted, along with a statement noting if the element will be retained, removed, or repurposed.



4. If the site of a building that has been identified as using the Suburban or Auto-Service typology is being substantially redeveloped (defined in Section 3), it shall meet the standards for the Commercial Typology.

#### 6.4.2. Building Color

- a. A minimum combination of three (3) colors shall be required per Commercial Type building: one for the main body, another for the trim and the third one for doors or other architectural features. A minimum of two (2) and a maximum of four (4) colors are required for Residential Type buildings. Natural materials such as brick, aluminum, stone, count as one of the three colors. See Appendix E for preferred colors by style.
- b. Bands of color around the building are not allowed, unless they are consistent with the pre-approved colors and highlight an architectural feature.
- c. An applicant that meets the statutory criteria of Section 553.79(20)(a), Florida Statutes, must clearly identify in the application materials the specific elements of the proposed project that apply to the statute. Once identified, these statutory design elements will be approved administratively.
- d. Applicants may submit non-pre-approved colors for review and approval by the Corridor Review Committee. Approved colors shall apply to all future projects and shall amend the approved colors noted in Appendix E .
- e. Building elevations (secondary/interior side façades) shall have the same color and materials as the façade facing the street.

Figure 13. Examples of Color Highlighting Architectural Features



Allowed – Left: Brick counts as one of the three colors; Right Door and awnings provide accent colors



Prohibited – Monochromatic schemes are not allowed.



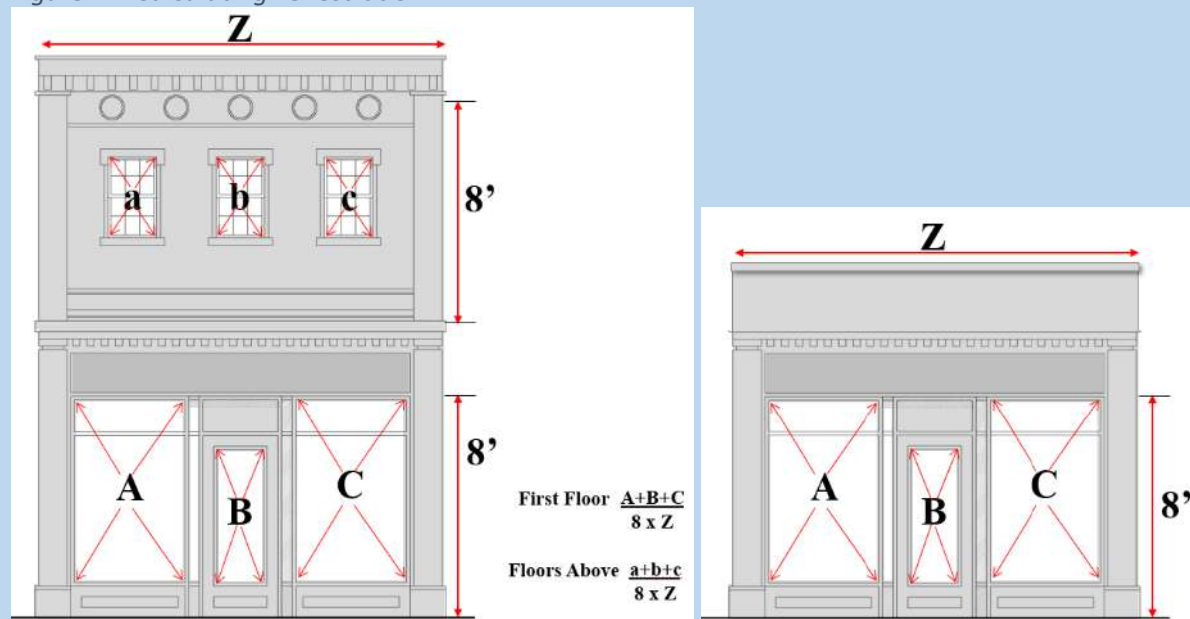
### 6.4.3. Building Materials

Allowable exterior finishes shall be selected based on the architectural style (see Appendix B). Exposed pre-engineered metal buildings, metal siding that is found on pre-engineered metal buildings, plastic panels, and other veneers are not allowed, unless a Modification of Standard is approved by the CRC. Unpainted wood siding is not allowed; however, natural siding (cedar, cypress, ipe, and natural hardwoods) may be left unpainted.

### 6.4.4. Fenestration and Glazing

*Fenestration* refers to the proportion of glass to the solid surface of a façade. *Glazing* refers to the transparency and reflectance of the glass. Fenestration shall be calculated as the total area of glass (windows and glass doors), or openings in the case of open porches, divided by the façade area measured from ground to eight (8) feet above ground in the case of the first story, and from the floor of the upper story to eight (8) feet above that line for upper stories (see Figure 14). If the façade includes several planes, they all count toward the calculation as long as the façade planes are within the required building setback.

Figure 14. Calculating Fenestration



1. For the Commercial Typology, the percent of fenestration shall be 30% minimum and 65% maximum of the ground floor facade facing the street. The percent of fenestration on upper floors facing the street shall be 20% minimum and 40% maximum.





Figure 15. Commercial Typology - Examples of Fenestration



Prohibited - Do not meet the minimum



Allowed – Meet the glazing rules

2. For the Residential Typology, the percent of fenestration shall be 20% minimum and 50% maximum for all floors facing the street.

Figure 16. Residential Typology - Examples of Fenestration



Allowed – Meet the glazing rules

3. The size and orientation of windows shall reflect the architectural style of the building (see Appendix B).
4. Glass shall be clear float (thin, smooth, clear glass surface), gray light, or solar glazing with 80% minimum transmittance. Reflective glass or film is not allowed.
5. Glass panes shall not exceed a maximum size of 6 feet by 8 feet.





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6. Windows shall not be obscured or visually blocked by paint, shelves or attachments, but blinds and curtains are allowed to achieve privacy.
7. Window sills and bulkheads shall extend a minimum of 12 inches and a maximum of 30 inches above the finished floor (see Figure 17). Bay doors reused as windows are exempt from meeting this requirement. Buildings that are required to be elevated due to floodplain regulations may measure these distances from the interior finished floor.

*Figure 17. Bulkheads/Window Sills*



#### 6.4.5. Awnings and Canopies

- a. Awnings or canopies (see Figure 18 & 19), if installed, are required between the first and second stories to provide protection from the weather and to delineate the transition between ground and upper floors.
- b. Awnings and canopies over windows are encouraged. However, they shall not obscure prominent architectural features, such as cornice lines, parapets, decorative window trim, etc. Stretched awnings on curved aluminum frames are not allowed.

*Figure 18. Example of Awning*





Figure 19. Examples of Canopies



- c. Awnings shall be placed in a way to accentuate the architectural features of the building. Therefore, awnings shall not be installed to extend over multiple windows and doors. Figure 20 shows two examples, one where the awning was placed entirely across the façade (discouraged), while the other example shows the awnings used to accentuate the windows (encouraged).

Figure 20. Awning Placement



#### 6.4.6. Building Entrances

1. The main entrance of all buildings shall be oriented toward San Marco Avenue. The design of the façade shall reinforce presence of main entrance. A secondary pedestrian entrance to the building may be provided from the rear parking area.
2. The front entrance (facing San Marco Avenue) shall be operable, clearly-defined and highly-visible. In order to emphasize entrances, they shall be accented by a change in materials around the door, recessed into the façade (alcove), or accented by an overhang, awning, canopy or similar feature.
3. Doors may be solid or glazed. The acceptable door materials include wood, painted steel, and painted aluminum.
4. Pedestrian connections from the public sidewalk and parking areas to the building entrance shall be provided. Buildings with entrances at a higher elevation than the public sidewalk shall create a transition within the setback (see Figure 21).

Figure 21. Pedestrian Transition



#### 6.4.7. Building Roof

- a. Buildings with flat roofs shall have a cornice treatment or a parapet. The cornice shall be at least eighteen (18) inches in height. Parapet shall be a minimum of two (2) feet in height measured from the exterior horizontal roof surface.
- b. The vertical dimension of a hip roof shall not exceed the height of the supporting walls (see right).
- c. Roof materials shall be light-colored or a planted surface (green roof).
- d. Mansard roofs shall be prohibited (see Figure 22); Y shall not be larger than X.

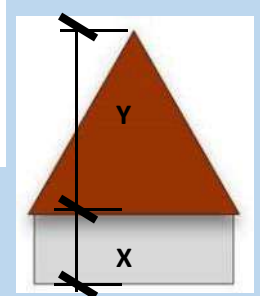


Figure 22. Examples of Mansard Roofs

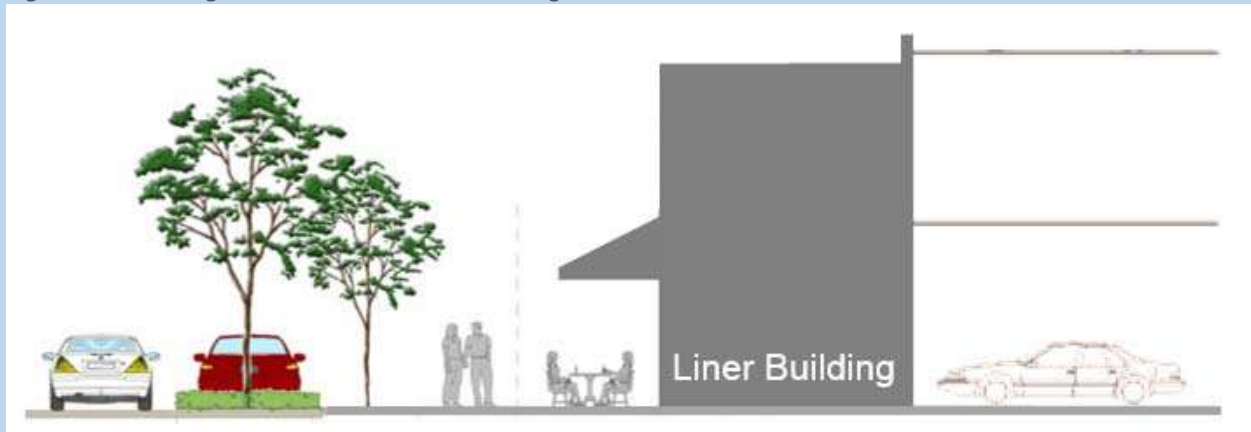


#### 6.4.8. Parking Structure Design

- a. Stand-alone parking garages, which are allowed in the CL-2 zoning district, shall require approval by the CRC.
- b. Parking structures accessory to a principal use are required to be placed behind a liner building that houses active uses (e.g. commercial, office, residential). The liner building, which may be attached or detached from the parking structure, may be one story but shall extend for a minimum of seventy-five (75) percent of the length of the parking structure, and shall have a minimum depth of thirty (30) feet. See Figure 23.



Figure 23. Parking Structure with Liner Building



- c. Any portion of the parking structure that is not concealed behind a building shall be screened to conceal all internal elements such as plumbing pipes, fans, ducts and lighting. Ramping should be internalized wherever possible. Exposed spandrels shall be prohibited.
- d. The portion of the parking garage not concealed behind the liner building shall provide a landscaping strip along the façade. The landscaping strip shall have a minimum depth of 5 feet.
- e. The exterior facades of the parking garage shall be designed to achieve architectural unity with the principal structure.
- f. Parking structures shall meet setback, height, façade articulation, fenestration and glazing standards contained in this section.
- g. Ventilation openings shall resemble typical fenestration found within a particular area.

## 6.5. Site Access and Parking

### 6.5.1. Pedestrian Access and Circulation

Pedestrian walkways, ramps or steps (or a combination of these) shall link the public sidewalk to the front door entry.

### 6.5.2. Vehicular Access

Parking areas shall be accessed from a secondary street, or from an adjacent property (shared use agreement necessary), if any of these are available or proposed as part of the development. Driveway connections to San Marco Avenue shall be allowed as a last resort and shall require the applicant to provide evidence to this fact. The following standards shall apply to all driveways:

- a. Only one driveway (ingress and egress) shall be allowed per site. A one way ingress and one way egress shall equal one driveway. Only if required by the Fire Marshal for safety, additional driveway(s) may be permissible.
- b. One-way drives shall not exceed 13 feet in width.





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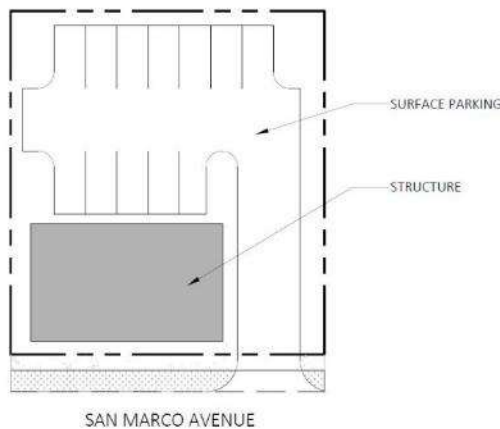
- c. Two-way driveways shall not exceed 24 feet in width.
- d. Turning radii along San Marco Avenue will be determined by the applicable authority in the right of way.
- e. Turning radii along all other streets shall not exceed 20 feet.

### 6.5.3. Parking Placement.

Surface parking lots shall be located behind the building (see Figure 24). However, the following parking placement alternatives are also allowed:

- a. Where a site is too shallow to provide parking behind the building, the placement of parking facilities is permitted on the side of the proposed building only if the building setback and building frontage requirements are met. A street wall and landscaping will be required to screen the parking facility.
- b. The placement of parking facilities in front of a building is only permitted for the portion of the building that is not used to meet the building setback and building frontage requirements. A street wall and landscaping will be required to screen the parking facility.

Figure 24. Parking Location and Access – Allowed



### 6.5.4. Existing Non-Conforming Parking Facilities

There are several sites in the district with parking spaces that do not meet current code standards in terms of access, location or size.

- a. *Parking in Front of the Building.* In conjunction with the redevelopment of a site, parking facilities that are already located in front of a building that is not being moved or demolished (see Figure 25) may remain.
- b. *Parking Spaces Accessed from Street.* In conjunction with the redevelopment of a site, existing parking spaces that are accessed directly from the street (as opposed to a parking lane) may remain as long as there is no option to provide the minimum parking requirements elsewhere on the site.
- c. *Non-Conforming Curb Cuts and Driveways.* Will only be required to meet the standards of this section if there is substantial redevelopment on the site.



Figure 25: Examples of Existing Non-Conforming Parking Facilities



### 6.5.5. Bicycle Parking

In conjunction with new construction and substantial redevelopment (as defined in Section 3), the applicant may provide bicycle parking facilities on the site at a rate of one bike rack space per 20 required vehicle spaces. The rack may be placed within the street setback area, but not within the public right-of-way, and it shall count towards the required front or side yard buffer (2' of width per bicycle space) *or* as an offset to required trees at a rate of one tree per three spaces (not to exceed two (2) trees).

### 6.6. Site and Building Lighting

Acceptable lighting on the site may include lights on the exterior of a building, up or down lighting shining on buildings or trees, and parking lot pole lighting.

- a. Lighting fixtures on the building shall be compatible with the architecture of the building.
- b. Full cut-off lighting fixtures shall be used for all exterior lighting. No lights are to glare directly at the right-of-way or the sky and no source of illumination shall be visible from a residential district.
- c. Permanent accent lighting designed to accentuate building details is encouraged.
- d. Exposed interior and exterior fluorescent or incandescent bulbs that are visible from the public right-of-way are not allowed. Only exterior cut sheets shall be required for submittal.
- e. Color LED and/or neon may be used as architectural lighting to accentuate building frame, profiles or details, with Corridor Review Committee approval.
- f. Lighting poles may not surpass 20' in height as measured to the point of light output (see Figure 26).
- g. LED dimming technology shall be incorporated that reduces lighting level to minimum safe illumination after business closing hours. Refer to Illuminating Engineering Society guidelines. Minimum for a private parking lot is 0.13 footcandles. Lighting professional shall confirm minimum lighting level.





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Figure 26. Examples of Lighting Heights





## 6.7. Stormwater Facilities

Stormwater retention facilities, where required, shall not be located within the front or street side setbacks and shall be incorporated into the overall design of the project as amenities.

Projects shall include at least two of the following low impact design concepts: rain water harvesting, green roofs, bio-swales, rain gardens, parking lot water capture and pervious pavement (pervious concrete, pervious pavers, and/or other pervious pavements recognized by the United States Green Building Council). The goal of encouraging the use of these mechanisms is to reduce stormwater runoff, capture contaminants closer to the source and reduce the use of potable water for irrigation and grey water activities.

*Figure 27. Examples of Stormwater Facility Design*

Sources: The Urban Report; Green and Sustainable Services, LLC.





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## Section 7. Landscaping and Screening

### 7.1. Landscaping Typologies

The landscape character of San Marco Avenue is largely dependent on the built typology of the structure (converted residential and traditional commercial).

This section contains the detailed landscaping standards for properties along San Marco Avenue. The quality, installation, mitigation, and maintenance requirements of the City Code shall apply (see Section 25-53 of the Code).

#### 7.1.1. Plant Materials

In general, site landscaping shall consist of native or naturalized plants that thrive in the St. Augustine climate and environment (see plant list in Appendix C). Plants shall be relatively drought tolerant to encourage the conservation of water.

#### 7.1.2. Protection of Existing Landscape

Effort shall be made to minimize disturbance of terrain around buildings or elsewhere on the site, thus reducing the possibility of destroying or damaging important landscape features or archaeological resources. Construction plans shall indicate areas of protection and shall have applicable notes regarding minimal disturbance of landscape elements to be preserved.

Along the corridor, existing oaks in good health shall be protected when possible. Oaks over 12" dbh to be removed shall have an additional 25% deficit applied to current tree mitigation calculations.

### 7.2. Landscape Requirements: Residential Typology

#### 7.2.1. Introduction

The residential typology is characterized by a large front lawn (fronting San Marco Avenue) along with large shade trees for canopy. Landscape reinforces the main entry point and other architectural features.

#### 7.2.2. Front Yard Zone Landscape

##### a. Landscape Bufferyard Special Conditions

The front yard shall be defined as the entire area between the front façade of the building and the property line. Note that required trees and shrubs may be placed anywhere within this zone (versus the limitation of 10' from property line) so that trees and shrubs can accentuate entry points and foundation plantings appropriate for a residential typology.



**b. Landscape Bufferyard Classification**

CM-2 zoned front yards, fronting San Marco Avenue as well as US 1 North (inside the San Marco Avenue Entry Corridor), shall have bufferyard classification ‘G’ according to City code. Specifically:

<b>Trees</b>	<b>Shrubs</b>
<b>5 trees per 100’ of frontage</b>	<b>25 shrubs per 100’ of frontage</b>

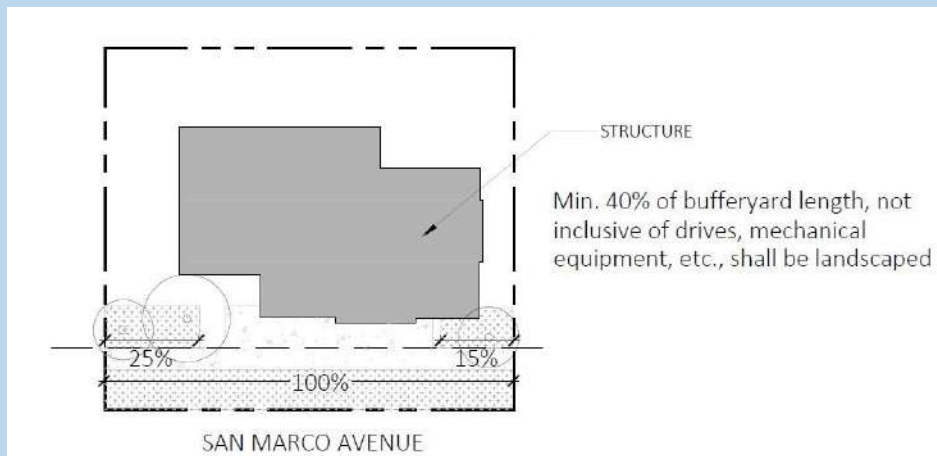
CL-2 zoned front yards, fronting San Marco Avenue, shall have bufferyard classification ‘F’ according to City code. Specifically:

<b>Trees</b>	<b>Shrubs</b>
<b>4 trees per 100’ of frontage</b>	<b>20 shrubs per 100’ of frontage</b>

**c. Percentage of landscape**

A minimum of 40% of the bufferyard length, not including drives, mechanical equipment, etc., shall be landscape. Raised planters are encouraged and shall count toward the bufferyard length; however, movable (potted) plants do not count toward a landscape bufferyard.

Figure 28. Landscape Percentage



**7.2.3. Side Yard Landscape**

**Landscape Bufferyard Classification**

- a. For both CM-2 and CL-2, yards shall have bufferyard classification ‘E’ according to City Code. Specifically:



Trees	Shrubs
4 trees per 100' of frontage	12 shrubs per 100' of frontage

- b. Screening buffers shall not be required between adjacent developed non-residential sites along San Marco Avenue; however, corner lots must provide landscaping on all street frontages.
- c. Vehicular Use Areas: Vehicular areas shall meet the following requirements in addition to the screening requirements set forth for the side and rear yards.
  - i. Within each parking area of five thousand (5,000) square feet or more, there shall be a minimum of five (5) percent of landscaped areas, which shall be reasonably distributed within each five thousand (5,000) square feet of parking area so as to be prorated to fulfill the objectives of this article.
  - ii. Each separate planting area shall contain at least one (1) tree and a tree shall be planted for each one hundred (100) square feet of internal landscaping. All internal landscaping shall be protected from vehicular encroachment by curbing or wheel stops. All landscape areas shall be a minimum of one hundred (100) square feet, with the least dimension being five (5) feet.

**7.2.4. Percentage of landscape**

A minimum of 70% of the bufferyard length, not including drives, mechanical equipment, etc., shall be landscape. Modification may be allowed if technically unfeasible.

Figure 29. Side Yard Percentage







### 7.2.5. Rear Landscape and Alleyway Bufferyard

#### Landscape Bufferyard Classification

- a. Rear yard shall have bufferyard classification ‘E’ according to City code for both traditional commercial and converted residential typologies. [Note: current code ‘D’] Specifically:

<b>Trees</b>	<b>Shrubs</b>
<b>4 trees per 100’ of frontage</b>	<b>12 shrubs per 100’ of frontage</b>

- i. When the rear yard setback is adjacent to a residential site: in addition to the 15 foot setback required in Section 6 and the landscaping required in Section 7, a 6-foot minimum (8-foot maximum) finished masonry wall (stucco, decorative segmental wall, brick) shall be constructed within the rear buffer yard of the site. The adjacent residential property owner(s) may request directly from the applicant a wooden fence or landscape-only buffer and such agreement shall be filed with the building permit application with a notarized letter(s).
  - ii. If the site use changes, the adjacent residents shall be contacted by the property owner to inquire if a wall is desired by the residents (if a wall was not already constructed).
- b. When any setback is adjacent to an alleyway with commercial zoning on both sides: the setback shall be bufferyard classification ‘D’ according to City code. [Note: current code ‘F’ for front yard / ‘D’ for rear yard] Specifically:

<b>Trees</b>	<b>Shrubs</b>
<b>3 trees per 100’ of frontage</b>	<b>4 shrubs per 100’ of frontage</b>

### 7.3. Landscape Requirements: Commercial Typology

#### 7.3.1. Introduction

The commercial typologies are characterized by limited landscape in the front. Landscape is often in raised planters and narrow strips when provided.

#### 7.3.2. Front Yard Landscape

##### a. Landscape Bufferyard Special Conditions

For commercial typologies, up to 50% of required landscape may be shifted to adjacent side yard.



**b. Landscape Bufferyard Classification**

CM-2 zoned front yards, fronting San Marco Avenue, shall have bufferyard classification ‘G’ according to City code. Specifically:

<b>Trees</b>	<b>Shrubs</b>
<b>5 trees per 100’ of frontage</b>	<b>25 shrubs per 100’ of frontage</b>

CL-2 zoned front yards, fronting San Marco Avenue, shall have bufferyard classification ‘F’ according to City code. Specifically:

<b>Trees</b>	<b>Shrubs</b>
<b>4 trees per 100’ of frontage</b>	<b>20 shrubs per 100’ of frontage</b>

**c. Percentage of landscape**

A minimum of 25% of the bufferyard length, not including drives, mechanical equipment, etc., shall be landscape. Raised planters are encouraged and shall count toward the bufferyard length; however, movable (potted) plants do not count toward a landscape bufferyard.

Figure 30. Landscape Percentage

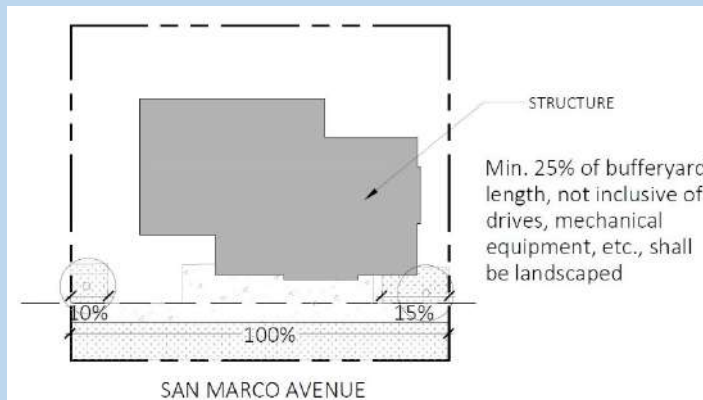


Figure 31. Examples of Planters





**7.3.3. Side Yard Landscape**

**a. Landscape Bufferyard Classification**

- a. Screening buffers shall not be required between adjacent non-residential sites along San Marco Avenue. [Note: current code 'E']
- b. When side yard setback is adjacent to a residential site:
  - i. Side yard shall have bufferyard classification 'E' according to City code. [Note: current code 'E'] Specifically:

<b>Trees</b>	<b>Shrubs</b>
<b>5 trees per 100' of frontage</b>	<b>20 shrubs per 100' of frontage</b>

- ii. When side yard setback is adjacent to a roadway: the side yard shall have bufferyard classification 'F' according to City code. [Note: current code 'E'] Specifically:
- iii. Up to 50% of required landscape may be shifted to a front, rear, or other side yard buffer.

<b>Trees</b>	<b>Shrubs</b>
<b>4 trees per 100' of frontage</b>	<b>12 shrubs per 100' of frontage</b>

- c. Screening buffers shall not be required between adjacent non-residential sites along San Marco Avenue.
- d. Vehicular Use Areas: Vehicular areas shall meet the following requirements in addition to the screening requirements set forth for the side and rear yards.
  - i. Within each parking area of five thousand (5,000) square feet or more, there shall be a minimum of five (5) percent of landscaped areas, which shall be reasonably distributed within each five thousand (5,000) square feet of parking area so as to be prorated to fulfill the objectives of this article.
  - ii. Each separate planting area shall contain at least one (1) tree and a tree shall be planted for each one hundred (100) square feet of internal landscaping. All internal landscaping shall be protected from vehicular encroachment by curbing or wheel stops. All landscape areas shall be a minimum of one hundred (100) square feet, with the least dimension being five (5) feet.



### 7.3.4. Percentage of landscape

A minimum of 70% of the bufferyard length, not including drives, mechanical equipment, etc., shall be landscape. Modification may be allowed if technically unfeasible.

Figure 32. Side Yard Percentage



## 7.4. Rear Landscape and Alleyway Bufferyard

### 7.4.1. Landscape Bufferyard Classification

- a. Rear yard shall have bufferyard classification 'E' according to City code for both traditional commercial and converted residential typologies. [Note: current code 'D'] Specifically:

Trees	Shrubs
4 trees per 100' of frontage	12 shrubs per 100' of frontage

- i. When the rear yard setback is adjacent to a residential site: in addition to the 15 foot setback required in Section 6 and the landscaping required in Section 7, a 6-foot minimum (8-foot maximum) finished masonry wall (stucco, decorative segmental wall, brick) shall be constructed within the rear bufferyard of the site. The adjacent residential property owner(s) may request directly from the applicant a wooden fence or landscape-only buffer and such agreement shall be filed with the building permit application with a notarized letter(s).
- ii. If the site use changes, the adjacent residents shall be contacted by the property owner to inquire if a wall is desired by the residents (if a wall was not already constructed).



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- b. When any setback is adjacent to an alleyway with commercial zoning on both sides: the setback shall be bufferyard classification 'D' according to City code. [Note: current code 'F' for front yard / 'D' for rear yard] Specifically:

<b>Trees</b>	<b>Shrubs</b>
<b>3 trees per 100' of frontage</b>	<b>4 shrubs per 100' of frontage</b>

#### 7.4.2. Landscape Percentage

A minimum of 70% of the bufferyard length, not including drives, mechanical equipment, etc., shall be landscape.

#### 7.5. Fences and Walls

While fences are typically used to achieve privacy on a site, street walls are used to continue the building frontage along a street, to screen vehicular areas such as parking lots, and to frame public zones such as courtyards and outdoor dining areas. Street walls and fences shall meet the following standards:

- a. In the absence of a building façade along any part of the San Marco Avenue site frontage line, a street wall shall be built in line with (co-planar with) the façade. Breaks are permitted in the street wall to provide vehicular and pedestrian access to the site, for plazas and gathering spaces, and for the purpose of tree protection. Street walls shall have openings no larger than necessary to allow automobile and pedestrian access.
- b. Street walls are the only types of fences/walls allowed facing streets (see Figure 33 and 34). (Privacy fences and walls are not allowed between adjacent uses along San Marco Avenue.)
- c. Street walls shall be located within the front setback, and may have landscaping or hard surface between the wall and the public sidewalk.
- d. Street walls shall be a minimum of 3 feet in height and shall not exceed a 5 foot maximum, as measured from the lower elevation of the public sidewalk or final elevation of adjacent interior development. However, the portion of the wall above 3 feet shall be no more than 50% solid.
- e. Street walls shall be constructed of wrought iron, brick, masonry, powder-coated aluminum or stone matching or complementing the finishes on the building. Chain link, wire, and PVC fencing shall be prohibited.
- f. The area in front of a street wall may be landscaped or used to expand the sidewalk.
- g. When landscaping is provided between the wall and the sidewalk, the landscaping strip shall be a minimum of two (2) feet wide.
- h. A three (3) foot wide landscape strip, as measured from the wall, shall also be provided on the interior side of the street wall.



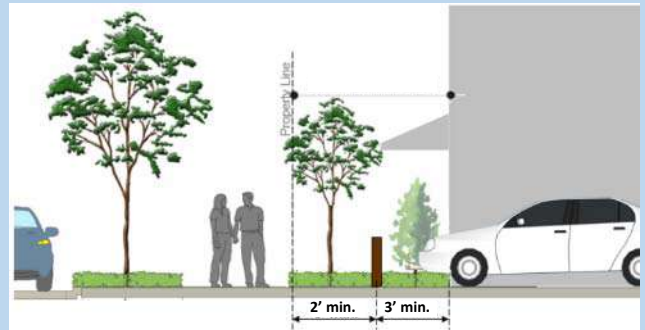
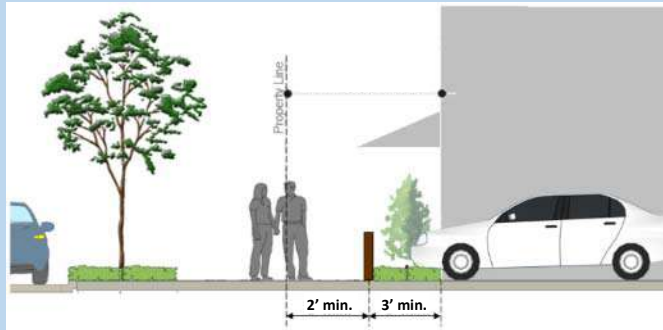
Figure 33. Examples of Street Walls



Figure 34. Street Walls and Landscaping

Expanded Sidewalk/Hardscape in front of Street Wall

Landscaping in front of Street Wall







## 7.6. Mechanical Equipment.

For the purpose of this section, mechanical equipment shall include, but is not limited to, a heating, ventilation, or air conditioning unit placed outside of a building, in addition to any utility-related equipment such as pumps, meters, transformers and similar equipment required to be installed on a development site. Mechanical equipment shall be located so that noise and visual impacts upon abutting residential property are minimized.

- a. Mechanical equipment shall not be permitted within any required building setback unless the utility company determines this to be the only possible location.
- b. Mechanical equipment shall not be located between the building and the street, unless technically infeasible. On corner lots, the location of mechanical equipment along a secondary street may be permitted through the modification of standards process (see Section 4.7) subject to the screening requirements stated below.
- c. Screening (by wall, solid fence or vegetative matter) that exceeds the height of the equipment shall be required if the equipment is visible from the street or adjacent properties. Certain equipment, such as, but not limited to rain barrels or cisterns, shall not require screening provided they are designed to blend with the building and are constructed with the same materials and colors.
- d. Air conditioning units and elevator shafts are permitted to go above the standard building height limit but shall be visually screened from all public rights-of-way as shown in Figure 35. This restriction may limit the number of stories that can be built within the 35 foot maximum building height.

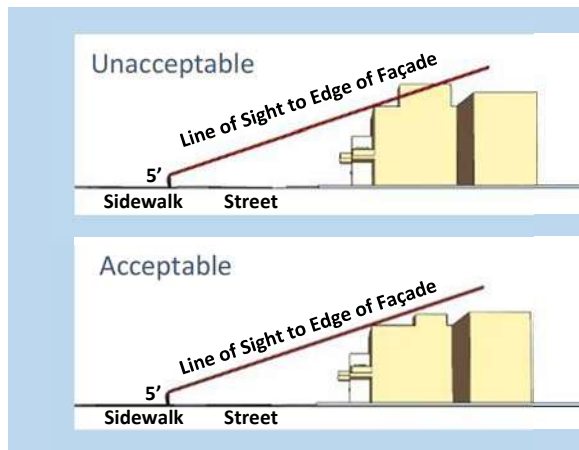


Figure 35. Equipment Screening

Screening rooftop equipment measured from furthest edge of right of way (Top Left: Unacceptable, Bottom Left: Acceptable).

## 7.7. Solid Waste Disposal Facilities

- a. Solid waste disposal facilities, solid waste, recycling, yard trash containers, and grease containers must be located to the side or rear of the building and shall be concealed from view from all sides by a wall or fence of up to 7 feet in height. The enclosure shall be made of wood, masonry or vinyl and shall be fitted with an opaque sliding or hinged door and working latch. Gates shall be able to be secured in the open position. No chain link fence is allowed for solid waste enclosures.



- b. On corner or double-frontage lots, the disposal facilities shall be accessed from the secondary frontage.
- c. Enclosure shall adhere to City Solid Waste requirements.
  - i. Width of 10-12 feet minimum.
  - ii. Depth of 10-12 feet minimum.
  - iii. Height of 7 feet minimum.
  - iv. Overhead clearance over and in front of dumpster shall be clear of obstructions 20' in height.
  - v. Bumper posts: two (2) steel, 4" diameter set in concrete. Placement 12" from back wall of enclosure.

### 7.8. Visibility Triangle

All landscaping within the triangular areas described below shall provide unobstructed cross visibility at a level between two (2) and six (6) feet above the ground. Trees having limbs and foliage extending into this cross visibility area shall be allowed, provided that they are so located so as not to create a traffic hazard. Landscaping, except grass and ground cover, shall not be located closer than three (3) feet from the edge of any accessway pavement. The triangular areas are:

- a. The areas of property on both sides of an accessway formed by the intersection of each side of the accessway, i.e. driveway, and the public right-of-way pavement line with two (2) sides of each triangle being ten (10) feet in length from the point of intersection and the third side being a line connecting the ends or the other sides.
- b. The area of property located at a corner formed by the intersection of two (2) or more public rights-of-way with two (2) sides of the triangular areas being measured thirty (30) feet in length along the abutting edges of pavement, from their point of intersection, and the third being a line connecting the ends of the other two (2) lines.



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## Section 8. Signs

Each development shall be allowed up to two (2) *building signs* per **business** with frontage along San Marco Avenue. Developments utilizing the Residential Typology are eligible to also erect a freestanding sign in addition to the building signs. Existing signs not meeting the requirements of this section are subject to the non-conforming provisions of the Code of Ordinances (see Section 3-23(k)).

### 8.1. Number and Types of Signs Allowed

- a. Two (2) business identification signs attached to the building shall be allowed per business frontage on a public street. The types of signs allowed are as follows (see Section 2, Definitions):
  - Wall Sign
  - Canopy Sign
  - Awning Sign
  - Projecting Sign
  - Hanging Sign
  - Window Sign
- b. The use of portable signs, streamers and pennants shall follow existing sign code.
- c. Blinking lights, bulbs facing the road, and electronic message signs are not permitted.

### 8.2. Building Sign Size

The total allowable size of all building signs combined shall be as follows, and may be distributed among the various building signs allowed. The maximum size of any building sign, however, shall be no more than one hundred twenty (120) square feet.

<b>Building/Business Frontage</b>	<b>Maximum Combined Sign Size</b>
0-49 feet	120 sq. ft.
50-99 feet	170 sq. ft.
100-199	220 sq. ft.
200 feet and over	250 sq. ft.

### 8.3. Sign Illumination

- a. Interior-illuminated cabinet or channel letter signs are not allowed. Signs may only be illuminated from an exterior light fixture or backlit to accentuate raised lettering or images.
- b. There shall be no illuminated signs facing a single family residence.

Figure 36. Permitted Sign Illumination



Figure 37. Prohibited Sign Illumination



## 8.4. Building Signs

- a. Wall signs shall be either a panel or individual letters applied to the wall, shall not project from the wall by more than 12 inches, shall not extend above the top of the wall where it is located, and in the case of two story buildings, it shall be placed between the first and second floor windows. Signs painted directly onto the façade are not allowed.
- b. Projecting signs may be read horizontally or vertically, may extend into the building setback zone, but not over the public right-of-way. They shall have a clearance of 8 feet from the ground and shall not exceed the height of the building wall where installed.
- c. Canopy signs may be applied to the face of the canopy and shall not exceed 50 percent of the width of the canopy. They are allowed to be placed fully or partially above or below the edge of the canopy, provided the sign consists of individual letters (as opposed to a panel). Canopy signs are also allowed to hang from the bottom of the canopy facing the street. An 8 foot clearance shall be provided.



Figure 38. Canopy Signs



- d. Awning signs shall not cover more than 30% of the vertical face (or near vertical part) of the awning.

Figure 39. Awning Signs



- e. Window signs (silk screen, vinyl letters, gold leaf, hand painted or neon) shall not occupy more than 25% of the glass window or door, and may be allowed for first and second story businesses.
- f. Hanging signs shall not count toward the maximum sign area allowed, provided it is placed under the awning or canopy, perpendicular to the building, and near the front door of the business. Such signs shall have an 8 foot minimum vertical clearance as measured from grade to the bottom of the sign, and a maximum height of 2 feet. While hanging signs do not count towards the maximum sign area, they do count toward the maximum allowed signs per business.
- g. Signs shall not obscure architectural details such as windows, cornice, decorative brickwork and storefronts.

## 8.5. Free Standing Signs

The need for freestanding signs is based on the placement of the building away from the street. When buildings cannot be seen by passing motorists, they need a larger sign by the street to indicate the location of the business. As future traditional commercial development in the San Marco Avenue corridor is expected to bring the buildings closer to the street, it will not be necessary to utilize freestanding signs to locate the businesses. The building itself, and the signs on the building, will be visible to the passing motorists.

The Residential Typology, on the other hand, will continue to place the buildings at a certain distance from the street. As an incentive for preserving this form of development, freestanding *pedestrian pole signs* will be allowed provided they meet the following standards:





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- a. The sign shall not exceed a height of fifteen (15) feet measured from the adjacent sidewalk and shall provide a clearance of 8 feet from the ground if it overhangs a pedestrian or vehicular circulation area.
- b. Pedestrian pole signs may be supported by one or two poles, or may be suspended from the structure as shown in Figure 40. The supporting poles may not exceed a width of 6 inches (each).
- c. The sign shall not be placed within 5 feet from any portion of the building.
- d. The sign may be placed up to the front property line, but shall not extend over the public sidewalk (far right below).

*Figure 40. Examples of Pedestrian Pole Signs (Note that all new signs shall not extend over the public sidewalk)*





## APPENDIX A: ENTRY CORRIDOR SITES

### Criteria:

- 1.) Sites with frontage along San Marco Avenue and/or US 1 (east) north of Picolata Road.
- 2.) 150' from the San Marco Avenue right-of-way shall be applicable.
- 3.) If 150' is over 70% of entire parcel, all of parcel is applicable.
- 4.) Accessory uses and structures, included but not limited to parking on adjacent or nearby sites, shall be subject to the San Marco Avenue Design Standards.

The map of sites (at right) indicates in blue if entire parcel falls under the Design Standards, or red if 150' impact zone is applicable.





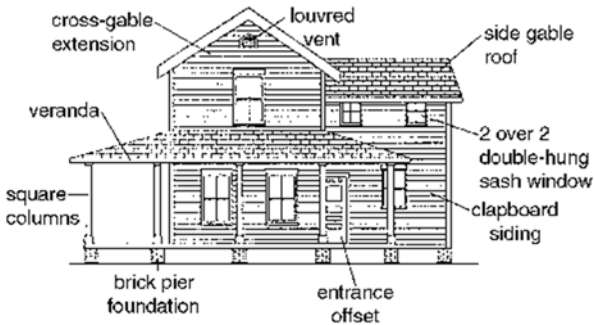
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## APPENDIX B: ARCHITECTURAL STYLES

### FRAME VERNACULAR



Images by others

### BACKGROUND

Frame Vernacular is one of the most dominant architectural styles in the San Marco Avenue area. Vernacular architecture refers to a regional or “folk” architecture, built with local materials and local labor, without formal plans, and for the most economical price at the time. Vernacular, while considered a style, does not belong to any particular formal architectural style, rather its design reflects a trend toward simplicity. Some Frame Vernacular buildings have one story, others have two; while some have front gable roofs, others may have side-gable or cross gable roofs.

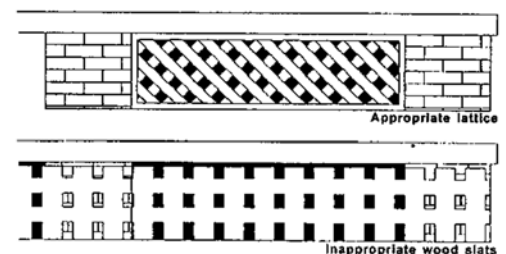
### BUILDING PLAN

- 1) Rectangular or L-shaped.
- 2) Garages and carports are secondary to the main building/facade. They are either recessed from main façade or detached and placed in the rear. Sites that have been converted to commercial or office use have demolished the garage (if one was present in the rear) and replaced it with surface parking.

### FOUNDATION

- 1) Brick, tabby or concrete block piers.
- 2) Spaces between piers left open.
- 3) Lattice infill between piers is common.

### FACADES





- 1) Vertically oriented.
- 2) Three-bay or five-bay pattern.
- 3) Simple entrance.

## COLORS

- 1) Colors range in the pastel family from light yellows and grays to light pastel colors.

## PORCHES

- 1) Wide front porches.
- 2) Full width, wrap around, or fill in between the "L".
- 3) Commonly elevated 2'-6" to 3'-6" above grade.
- 4) Columns are typically narrow and made of wood with few details; usually spaced evenly across the façade.
- 5) If railings used, they were wooden with 1 ¼ inch square balustrades.

## ROOF

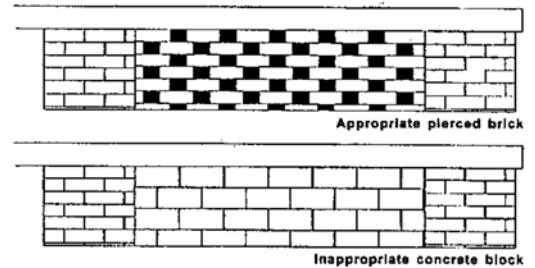
- 1) Front, side and cross gable.
- 2) Main roofs are steeply pitched (8:12 to 12:12).
- 3) Porch roofs have a low pitch (2:12 to 4:12).
- 4) Rafter ends are exposed, unadorned, and extend beyond the face of the wall.
- 5) Asbestos, composition or metal shingles, V-crimp metal.
- 6) Brick or stucco chimneys, typically on the gable end walls.
- 7) Dormers sometimes used on 1 ½ story homes.

## DOORS

- 1) Transoms, fanlights are common.
- 3) Door trim projects out from wall cladding, approximately ¾ inch. Jamb trim is at least 4" wide, and headers (lintel) are normally 6" wide.
- 4) Doors contain recessed wood panels.

## WINDOWS

- 1) Windows are single, tall and narrow.
- 2) Multi-pane, double-hung sash windows (6/6, 3/1, 2/2 or 1/1). Jalousie used in later construction (mid-century).
- 3) Transoms, fanlights and attic louvers are common.
- 4) Second floor windows align with first floor windows.
- 5) Houses built in the early 1900s had wood windows. Aluminum and vinyl windows have been used more recently.
- 6) Windows are spaced evenly along all facades.
- 7) Window trim projects out from wall cladding, approximately ¾ inch. Jamb trim is at least 4" wide, and headers (lintel) are normally 6" wide.







- 8) Shutters are not typically used. When used, they should be paneled and should be either operable or proportioned to look operable.

## MATERIALS

- 1) Horizontal wood siding; less common are wood shingles and board and batten. Vinyl and fiber cement siding used more recently.

## ORNAMENTATION

- 1) Sparse, limited to ornamental woodwork.

## FENCES

- 1) Wooden fences are most common.
- 2) Picket fences with various level of ornamentation, consistent with the main building.

## RESIDENTIAL ARCHITECTURE





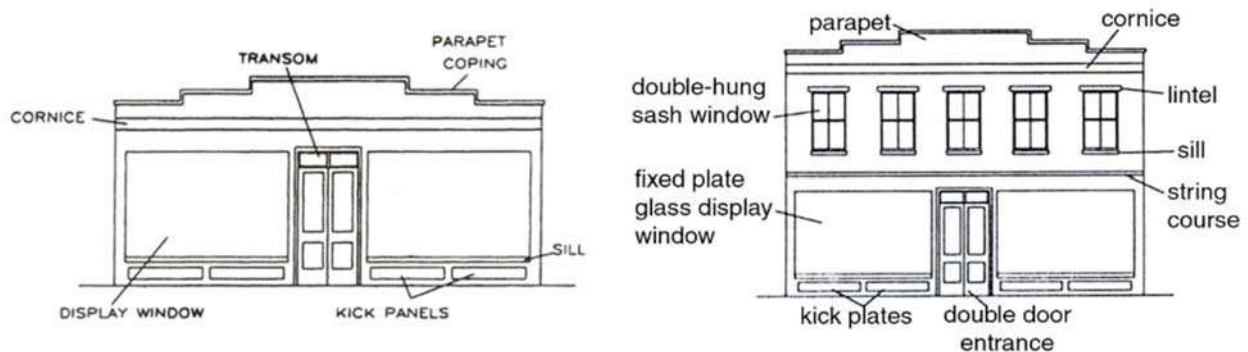


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## TRADITIONAL COMMERCIAL



Images by others

### BACKGROUND

Generally, a one-story building formed by a structural framework consisting of columns, bulkheads or kick-panels, and a cornice topped by a parapet. Large show windows generally placed within this framework to display merchandise and light the interior. The wall area between the windows and the cornice provide a place for advertising and makes the façade appear taller. In the case of multi-story commercial buildings, it is organized into an upper and lower zone. The design of the lower zone was essentially the same as the one story building. The lower zone generally houses commercial uses, while the upper zone often provided space for private uses, such as apartments, offices, hotel rooms, and meeting halls.

### BUILDING PLAN

- 1) Rectangular plan.
- 2) One to two stories in height.

### FOUNDATION

- 1) Continuous concrete slab or brick foundation.

### FAÇADE

- 1) Narrow front elevation facing the street.
- 2) Focus of the design. Provides the building's identifying features.
- 3) Different framework in upper and lower stories.

### ROOF

- 1) Flat with parapet.

### MATERIALS

- 1) Primary exterior material is brick with common or running bond.
- 2) Rough textured stucco.



## WINDOWS AND DOORS (STOREFRONT)

- 1) Fixed plate glass display windows and doors on the first floor.
- 2) Double-hung sash windows in the upper floors.

## COLORS

- 1) Exposed or painted brick with colors ranging from earth tones to pastels.

## ORNAMENTATION

- 1) Awnings, canopies.
- 2) Cornice.
- 3) Cast concrete detailing.
- 4) Decorative brick work, such as corbeling.

## FENCES

- 1) No fences should be placed in the front between the right of way and the building face.

## COMMERCIAL BUILDINGS\*



\*These images present existing commercial buildings located on San Marco Avenue. Please note that awnings should seek to emphasize windows instead of the entirety of the façade.



## APPENDIX C: LANDSCAPE PLANTS

*Botanical Name*                      *Common Name*

### Street Trees

<i>Quercus laurifolia</i>	Laurel Oak
<i>Quercus virginiana</i>	Live Oak
<i>Quercus shumardii</i>	Shumard Oak
<i>Plantanus occidentalis</i>	Sycamore
<i>Taxodium distichum</i>	Bald Cypress
<i>Ulmus parrifolia</i>	Drake Elm

### Street Palms

<i>Phoenix canariensis</i>	Canary Island Date Palm
<i>Phoenix dactalifera</i>	Date Palm
<i>Phoenix sylvestris</i>	Sylvestris Date Palm
<i>Sabal palmetto</i>	Sabal Palm
<i>Washingtonia robusta</i>	Washington Palm

**Note: palms in blue required for north section of Anastasia Boulevard.**

### Accent Trees and Palms

<i>Acer rubrum</i>	Red Maple
<i>Bambusa spp.</i>	Bamboo (clumping)
<i>Butia capitata</i>	Pindo Palm
<i>Cercis cananadensis</i>	Redbud
<i>Ilex cassine</i>	Dahoon Holly
<i>Ilex opaca</i>	American Holly
<i>Ilex vomitoria</i>	Yaupon Holly
<i>Juniperus silicicola</i>	Southern Red Cedar
<i>Juniperus virginiana</i>	Eastern Red Cedar
<i>Lagerstroemia indica</i>	Crape Myrtle
<i>Ligustrum spp.</i>	Ligustrum
<i>Liquidambar styraciflua</i>	Sweetgum
<i>Livistonia chinensis</i>	Chinese Fan Palm
<i>Magnolia grandiflora</i>	Southern Magnolia
<i>Myrica cerifera (Morella cerifera)</i>	Wax Myrtle
<i>Phoenix reclinata</i>	Senegal Date Palm
<i>Prunus agustifolia</i>	Chickasaw Plum
<i>Prunus calleryana</i>	Bradford Pear
<i>Prunus caroliniana</i>	Cherry Laurel
<i>Trachycarpus fortunei</i>	Windmill Plum

*Botanical Name*                      *Common Name*

### Hedges\*, Shrubs\*, Cycads and Palms

(\*To be used as hedge or screen for parking lots.)

<i>Abelia grandiflora*</i>	Abelia
<i>Camellia japonica</i>	Camellia
<i>Camellia sasanqua</i>	Sasanqua Camellia



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<i>Cassia spp.</i>	Cassia
<i>Cortaderia selloana</i>	Pampas Grass
<i>Crinum spp.</i>	Crinum Lily
<i>Eleagnus pungens*</i>	Silverthorn
<i>Eragrostis spectabilis</i>	Purple Lovegrass
<i>Feijoa sellowiana*</i>	Feijoa
<i>Galphimia (or Thryallis) glauca</i>	Thryallis
<i>Gardenia jasminoides</i>	Gardenia
<i>Hibiscus spp.</i>	<i>Hibiscus</i>
<i>Ilex cassine</i>	<i>Dahoon Holly</i>
<i>Ilex cornuta*</i>	<i>Chinese Holly</i>
<i>Ilex cornuta* "Burfordi"</i>	<i>Burford Holly</i>
<i>Ilex crenata</i>	<i>Japanese Holly</i>
<i>Ilex vomitoria "nana"</i>	Dwarf Yaupon Holly
<i>Illicium anisatum*</i>	Star Anise
<i>Illicium parviflorum*</i>	Yellow Anise
<i>Juniperus chinensis*</i>	Chinese Juniper
<i>Juniperus spp.</i>	Junipers
<i>Lantana camara</i>	Lantana
<i>Ligustrum japonicum*</i>	Japanese Privet
<i>Magnolia grandiflora 'Little Gem'</i>	Little Gem Magnolia
<i>Muhlenbergia capillaris</i>	Muhly Grass
<i>Nandina domestica</i>	Heavenly Bamboo
<i>Nerium oleander</i>	Oleander
<i>Philodenron selloum</i>	Split Leaf Philodendron
<i>Photinia fraseri</i>	Fraser's Photinia
<i>Pittosporum tobira*</i>	Pittosporum
<i>Plumbago auriculata</i>	Plumbago
<i>Podocarpus macrophyllus*</i>	Podocarpus
<i>Pyracantha coccinea</i>	Firethorn Pyracantha
<i>Raphiolepis indica</i>	Indian Hawthorn
<i>Rhapidothylum hystrix</i>	Needle Palm
<i>Rhododendron indica*</i>	Indica Azaleas
<i>Rhododendron obtusum</i>	Dwarf Azaleas
<i>Rhododendron kurume</i>	Kurume Azaleas
<i>Serenoa repens</i>	Saw Palmetto
<i>Spartina spp.</i>	Cordgrass
<i>Thuja occidentalis</i>	American Arborvitae
<i>Viburnum odoratissium*</i>	Sweet Viburnum
<i>Viburnum suspensum*</i>	Sandankwa Viburnum
<i>Viburnum Tinus</i>	Laurustikus Viburnum
<i>Yucca elephantipes</i>	Spineless Yucca
<i>Yucca smalliana</i>	Bear Grass
<i>Zamia floridana</i>	Coontie
<i>Zamia furfuracea</i>	Cardboard Plant

**Groundcovers**

<i>Aspidistra elatior</i>	Cast Iron Plant	<i>Catharanthus roseus</i>	Periwinkle,
<i>Vinca Cyrtomium falcatum</i>	Holly Fern		
<i>Dietes iridioides</i>	African Iris		
<i>Ficus pumila</i>	Creeping Fig		
<i>Gaillardia pulchella</i>	Gaillardia		
<i>Gelsemi sempervirens</i>	Carolina Jessamine		
<i>Gerbera jamesonii</i>	Gerbera Daisy		
<i>Hedera canariensis</i>	Algerian Ivy		
<i>Hedera helix</i>	English Ivy		
<i>Hemerocallis spp.</i>	Day Lilly		



<i>Helianthus debilis</i>	Beach Sunflower
<i>Ipomoea stolonifera</i>	Beach Morning Glory
<i>Iris virginica</i>	Blue Flag
<i>Lantana depressa</i>	Yellow Pineland Lantana
<i>Lantana montevidensis</i>	Weeping Lantana
<i>Liriope spp.</i>	Lilly Turf
<i>Lonicera japonica "Halliana"</i>	Hall's Honeysuckle
<i>Nandina domestica "nana"</i>	Dwarf Heavenly
<i>Nephrolepis exaltata</i>	Sword or Boston Fern
<i>Ophiopogon japonicus</i>	Mondo Grass
<i>Pittosporum tobira "nana"</i>	Dwarf Pittosporum
<i>Rudbeckia hirta</i>	Black Eyed Susan
<i>Trachelospermum asiaticum</i>	Confederate Jasmine
<i>Trachelospermum jasminoides</i>	Star Jasmine
<i>Tulbaghia violacea</i>	Society Garlic
<i>Vinca major</i>	Bigleaf Periwinkle
<i>Zamia floridana</i>	Coontie

**Botanical Name**                      **Common Name**

**Vines**

<i>Clematis dioscoreifolia</i>	Japanese Clematis
<i>Clematis virginiana</i>	Sweet Autumn Clematis
<i>Glesmium sempervirens</i>	Carolina Jasmine
<i>Hedera canariensis</i>	Algerian Ivy
<i>Hedera helix</i>	English Ivy
<i>Ipomoea stolonifera</i>	Beach Morning Glory
<i>Jasminum multiflorum</i>	Downy Jasmine
<i>Jasminum sambac</i>	Arbian Jasmine
<i>Lonicera japonica "halliana"</i>	Hall's Honeysuckle
<i>Lonicera sempervirens</i>	Coral Honeysuckle
<i>Parthenocissus quinquefolia</i>	Virginia Creeper
<i>Trachelospermum asiaticum</i>	Confederate Jasmine
<i>Trachelospermum jasminoides</i>	Star Jasmine
<i>Wisteria sinensis</i>	Chinese Wisteria

**Turfgrass**

Bahia Grass Bermuda Grass Centipede Grass  
St. Augustine Grass  
Zoysia Grass

**Plants Not Listed**

All native plantings as defined by UF/IFAS or USDA to northeast Florida may be used in addition to this list.

All plantings defined as "Florida Friendly" and suited to northeast Florida's climate by UF/IFAS may also be used.

Refer to the Florida-Friendly Landscape website for more assistance: <http://fyn.ifas.ufl.edu/>





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**APPENDIX D: LEGACY BUILDINGS**



Buildings marked in pink are deemed Legacy Buildings by the COSA Historic Preservation Officer based upon the presence of traditional building materials and original form and plan of building is greater than or equal to 50 years old. For addresses, see the list included on the following pages.



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## Address

### WESTSIDE

58 San  
Marco  
Avenue



60 San  
Marco  
Avenue



74 San  
Marco  
Avenue







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76 San  
Marco  
Avenue



82 San  
Marco  
Avenue



102 San  
Marco  
Avenue



138 San  
Marco  
Avenue





154 San  
Marco  
Avenue



172 San  
Marco  
Avenue



208 San  
Marco  
Avenue



210 San  
Marco  
Avenue







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214 San  
Marco  
Avenue



236 San  
Marco  
Avenue



EASTSIDE

31 San  
Marco  
Avenue





**35 San  
Marco  
Avenue**



**39 San  
Marco  
Avenue**



**41 San  
Marco  
Avenue**





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**45 San  
Marco  
Avenue**



**47 San  
Marco  
Avenue**



**63 San  
Marco  
Avenue**







67 San  
Marco  
Avenue



73 San  
Marco  
Avenue



75 San  
Marco  
Avenue



77 San  
Marco  
Avenue





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**81 San  
Marco  
Avenue**



**147 San  
Marco  
Avenue**



**155 San  
Marco  
Avenue**



**195 San  
Marco  
Avenue**





197 San  
Marco  
Avenue



239 San  
Marco  
Avenue



299 San  
Marco  
Avenue







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**APPENDIX E: COLORS**



SW 0036  
**Buckram Binding**  
Interior



SW 0035  
**Indian White**  
Interior / Exterior



SW 0037  
**Morris Room Grey**  
Interior / Exterior



SW 0040  
**Roycroft Adobe**  
Interior / Exterior



SW 0041  
**Dard Hunter Green**  
Interior / Exterior



SW 0043  
**Peristyle Brass**  
Interior / Exterior

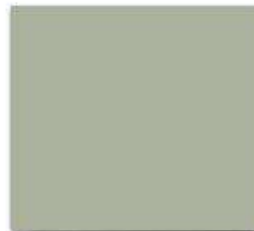


SW 0048  
**Bunglehouse Blue**  
Interior / Exterior

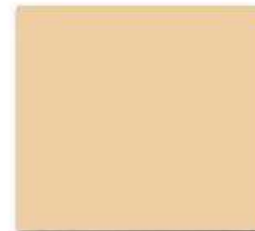
NOTE: Colors may be increased or decreased in saturation 10% from color sample.



SW 0065  
**Vogue Green**  
Interior / Exterior



SW 0066  
**Cascade Green**  
Interior / Exterior



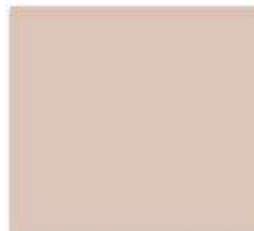
SW 0067  
**Belvedere Cream**  
Interior / Exterior



SW 0068  
**Copen Blue**  
Interior / Exterior



SW 0069  
**Rose Tan**  
Interior / Exterior



SW 0070  
**Pink Shadow**  
Interior / Exterior



SW 0071  
**Orchid**  
Interior / Exterior



SW 0072  
**Deep Maroon**  
Interior



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SW 6316  
**Rosy Outlook**  
Interior / Exterior



SW 6771  
**Bathe Blue**  
Interior / Exterior



SW 6464  
**Aloe**  
Interior / Exterior



SW 6815  
**Awesome Violet**  
Interior / Exterior



SW 0074  
**Radiant Lilac**  
Interior / Exterior



SW 0025  
**Rosedust**  
Interior / Exterior



SW 6035  
**Gauzy White**  
Interior / Exterior



SW 7083  
**Darkroom**  
Interior / Exterior



SW 6401  
**Independent Gold**  
Interior / Exterior



SW 6556  
**Obi Lilac**  
Interior / Exterior

NOTE: Colors may be increased or decreased in saturation 10% from color sample.



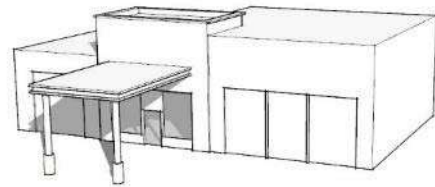
## APPENDIX F: GRANDFATHERED TYPOLOGIES

The following typologies are defined for use on existing buildings meeting this description. All other building types and new construction may not replicate or construct buildings in this typology.

### Auto Service Type

The Auto Service Typology reflects a few small scale gas stations and vehicle repair buildings along San Marco Avenue, which in some cases date back to the 1930s and are a character feature of San Marco Avenue. This architectural typology is not suitable or permissible for new development, and is only applicable when reusing or repurposing existing auto service type buildings.

The existing buildings are placed at a distance from the sidewalk, to allow vehicles to easily enter and exit the site. The presence of automobiles in front of the building (circulating or parked) is not conducive to pedestrian activity in the corridor. However, the continued use and the conversion of the existing buildings to accommodate other uses permitted in the zoning district will be allowed in order to preserve the existing buildings. When conversions are proposed, the main features of this type of development such as the large bay openings (may be converted to large window openings) will need to be preserved, but the vehicle activity in the front will be limited. Additions to the front of the building will not be allowed, as they would detract from the historic character of such buildings.



*Buildings designed to facilitate automobile access to bays (building far from the sidewalk, one story, repair bay doors, canopy)*

### Mid-Century Lodging

The Mid-Century Lodging typology reflects a few existing motels along San Marco Avenue, which were very common in Florida in the 1950s. The city intends to encourage their preservation and rehabilitation because they are a character feature of San Marco Avenue.



This architectural typology is not suitable or permissible for new development in the San Marco corridor and is only applicable when reusing or repurposing existing mid-century lodging type buildings. New lodging developments shall choose the Residential or Commercial Development typologies.

The features of this typology that should be preserved include the courtyard site layout with one or two stories of rooms with access from the courtyard. The buildings typically surround a central area that includes parking and amenities (swimming pool).



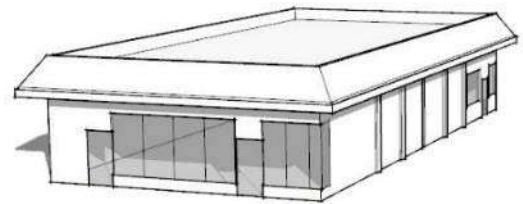
City of St. Augustine  
**San Marco Avenue Design Standards**



*Buildings originally designed using an auto-oriented design (building wrapped around a central courtyard used for parking and swimming pool)*

**Suburban Type**

The Suburban typology also reflects an existing type of development which started appearing in the San Marco Avenue corridor in the 1970s, prior to the adoption of design standards for the entry corridors. This type of development is not compatible with the urban, pedestrian-friendly character of the San Marco Avenue corridor. Therefore, this typology shall not be used for new development along the corridor. It is mentioned as one of the typologies found in the district for the purpose of guiding future additions and modifications to existing sites that already display this design. Redevelopment on these sites shall follow one of the other typologies allowed.



Developments in this typology consist of small buildings (relative to the size of the lot) placed at a large distance from the public sidewalk to allow for parking in front of the building. The architectural styles are very simple with no ornamentation or quality of design, large windows and mansard roofs. These buildings, because of their placement far from the street, rely on large freestanding signs in the front yard.

*Buildings originally designed using a suburban typology (building far from the sidewalk, parking in front of the building, one story)*



June 25, 2018

**Prepared by**



**Marquis Latimer + Halback, Inc.**

34 Cordova Street, Suite A

St. Augustine, FL 32084

**Jeremy Marquis**, RLA, ASLA, LEED AP BD+C, Project Manager

[jeremy@halback.com](mailto:jeremy@halback.com)



**S&ME, Inc.**

1615 Edgewater Drive, Suite 200

Orlando, FL 32804

**Pat Tyjeski**, AICP

[ptyjeski@smeinc.com](mailto:ptyjeski@smeinc.com)



