

ST. AUGUSTINE THE BRIDGE OF LIONS

SECTION 1: DATA GATHERING & SITE ANALYSIS OF PARK APPROACHES

REPORT JANUARY 2001

PREPARED FOR THE DISTRICT 2 FLORIDA DEPARTMENT OF TRANSPORTATION

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Acknowledgements

Herbert•Halback, Inc. would like to thank Charles Tingley and Leslie Wilson at the St. Augustine Historical Society Research Library for their assistance in gathering all of the historical photographs and periodical references for this document.

Background

Intent of the Bridge of Lions Rehabilitation Project

Efforts of the Bridge of Lions Rehabilitation Project not only include restoring the historical aesthetic of the bridge's original 1927 period architecture, but also improving the use value and safety of its structural components. The bridge's adjacent parks, located on the east and west "touch down" points of the bridge, as well as the bridge itself, are included in this overall rehabilitation effort.

The primary purpose of this report, as a subset of the larger rehabilitation effort, is to provide a comprehensive source for understanding the past and present conditions of the bridge's adjacent park system. The data gathering and analysis of the bridge's urban context and landscape architecture, included in this report, will provide the foundation for developing the adjacent park's urban design framework.

The following chapters serve as a compendium for the evolution of the bridge's urban context from the time of its original construction to present.



Undated aerial of St. Augustine inlet and harbor



Late 50's aerial of downtown St. Augustine - view of road widening and expansion of seawall along Avenida Menendez



Late 50's aerial of downtown St. Augustine

Purpose of the Data Gathering & Site Analysis

The following goals outline the intent of this report's data gathering and site analysis process:

- Provides thorough visual documentation of the park approaches' current conditions prior to physical impacts of bridge site work
- Depicts the evolution of historical events as important context for decision-making and design process
- Ascertains current conditions of site features and plant materials for both park approaches to determine historical appropriateness and feasibility for rehabilitation
- Provides the necessary background to assist in building public consensus as well as in gaining public trust and support for the concept development process
- Establishes the framework for the final design solution of the bridge's park approach system



View over Anderson Circle of bridge under construction, July 1925



East side marsh prior to bridge construction, August 1925

The Original 1927 Bridge Construction

Events Leading to and During 1927 Bridge Construction

Prior to construction of what is now known as the Bridge of Lions, an old wooden toll bridge served as the only access to Anastasia Island from downtown St. Augustine. This wooden bridge, built in 1895, had been renovated in 1904 to provide electric trolley service. Because of increasing use of automobile traffic, and because the wooden bridge had become such an eyesore thirty years after its construction, St. Augustine was ready for a new bridge. A leading sponsor for the bridge's construction was a young woman by the name of Miss Jean Rodenbaugh, the father of whom, H.N. Rodenbaugh, was vice president of the Florida East Coast Railway.

Appearing in the St. Augustine Record on January 23, 1925 was an announcement that "the city commissioners engaged J.E. Greiner and Company, Consulting/Engineers, to make preliminary estimates and surveys" for the construction of a bridge to span Matanzas Bay.

The article continues with the following description of the proposed bridge's adjacent parks:

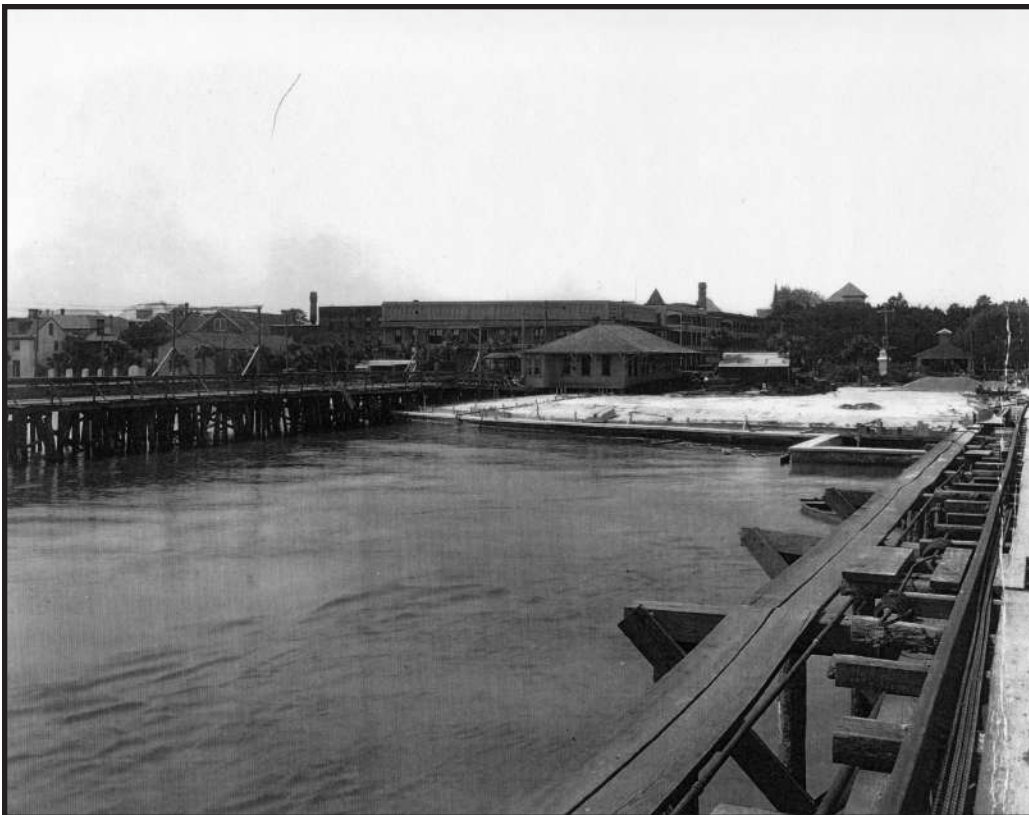
"At the St. Augustine end of the bridge it is proposed to construct a park which will practically be an extension of the existing plaza in the neighborhood of Cathedral Place. This plaza should be plentifully covered with suitable Palmetto trees which make very attractive parks. It will be on filled ground with natural slopes to the river bed, and it is proposed to protect these slopes with slabs of concrete or of local rock. Ultimately the city will, in all probability, extend this river front park in both directions as an additional improvement to the town, Therefore it is not considered advisable to construct an extensive sea wall around this plaza at the present time. Where the plaza joins the bridge proper it is proposed to construct an open pavilion of an appropriate design on each side of the roadway for the use of the public, and on the plaza side of this pavilion we would suggest the placing of the very appropriate decoration of statuary which the late Dr. Andrew Anderson, in such a public spirited manner, offered as an embellishment to this beautiful bridge. This location is merely suggested. If the Commissioner should decide to call the structure the "Ponce de Leon Bridge" or the "Bridge of Lions," as suggested by the Doctor, it may be well to omit the pavilions and make the distinctive feature the lions themselves, and the proper location for them then would be the place where the pavilions are shown on the drawings. This will be a matter for the Commissioners to decide when the details of the designs are being worked out. It is also proposed to construct two boat landings at this plaza. The plaza development is generally in line with the suggestion shown on a plan furnished by City Manager Masters.

At the Anastasia Island end is suggested simply a filled approach between the end of the bridge to a connection with the existing boulevard, slopes of the fill to be protected against erosion by local rock or concrete slabs."

Though construction of a concrete wall to frame and protect the circular plaza had been completed on the west side by late August 1925, landscape construction of the plaza area itself was delayed. The passage below is an excerpt from an article that appeared in the St. Augustine Evening Record on August 6, 1925, describing a final attempt to clear the bridge's approach area of any lingering bayfront businesses.

"Following a meeting of the City Plan Board yesterday afternoon, city commissioners and the city manager informally discussed with members of the Plan Board the position taken by George W. Corbett, fish dealer, who thus far has refused to move off the bay front to make room for the circular plaza approach to the new bridge now under construction. It was plain that the city authorities are in a quandary and that at least some of the municipal officials hesitate to take steps leading to a forcible ejection..."

...However, it was pointed out by those present who wished to see the bay front free from all business, with the exception of hotels and apartment houses, that Colonel Youngberg, representing the war department, has given the city permit to build the bridge and the circular approach to it, and that the Corbett dock is in the way of the contractor in the creation of this park."



Bridge forming at west approach - southwest view, 1925



Forming of bulkhead at plaza of west park approach, July 1925



Bridge forming at west approach - northwest view, 1925



East park approach as marshland prior to construction, 1925



Layout of bridge pilings, March 1926



West abutment, March 1926



Precast concrete post and chain construction at west plaza, April 1926



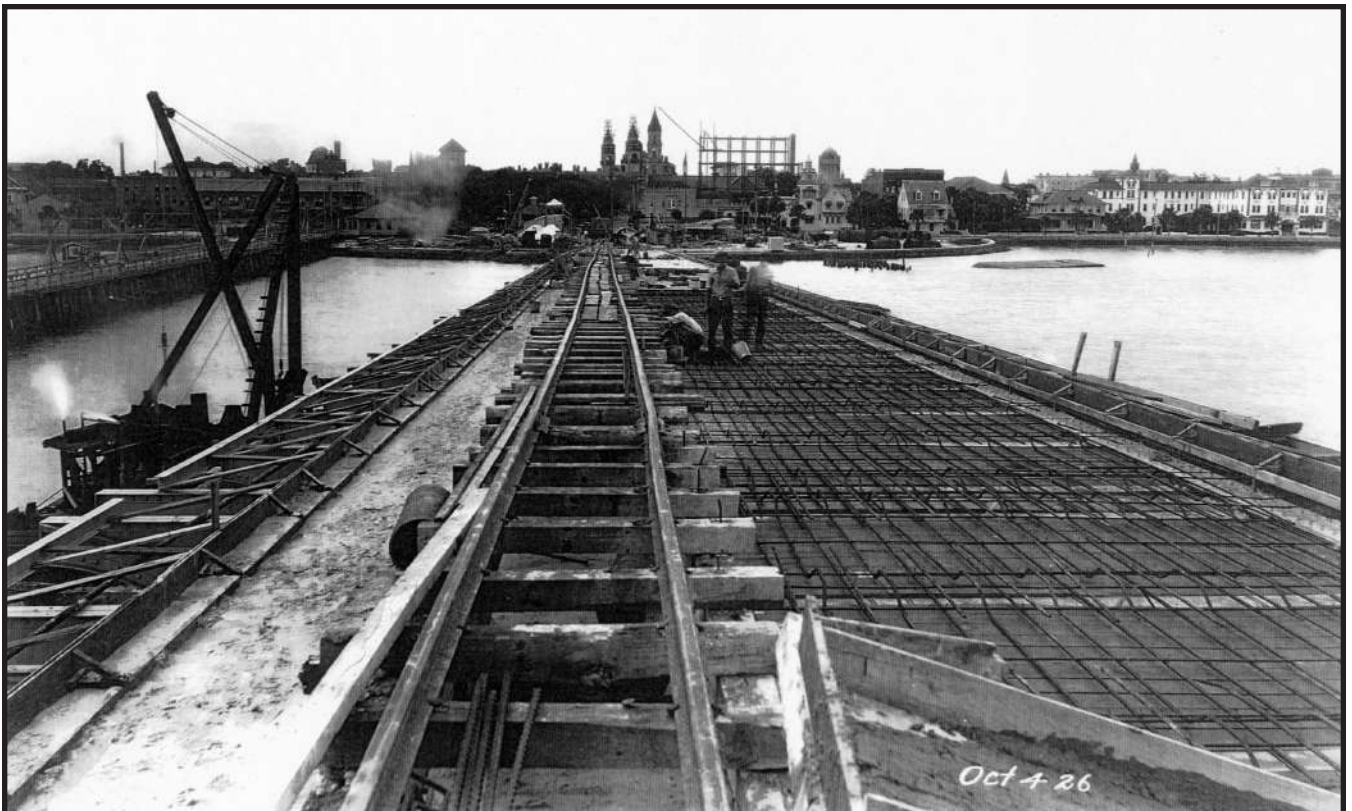
Forming of concrete piers - view east toward Davis Shores, May 1926



Construction of west abutment, September 1926



Bridge deck construction facing east, July 1926



Bridge deck construction facing west, October 1926



1927 view of bridge construction behind Ponce de Leon Circle



Construction of west bridge approach, January 1927

Construction Layout Plans of Bridge Park Approaches

The following fold-outs are excerpts from the original plans drawn up by J.E. Greiner and Company in 1925. Included in the fold-outs are the design development plans and construction plans for the west and east park approaches, as well as details of proposed site features.

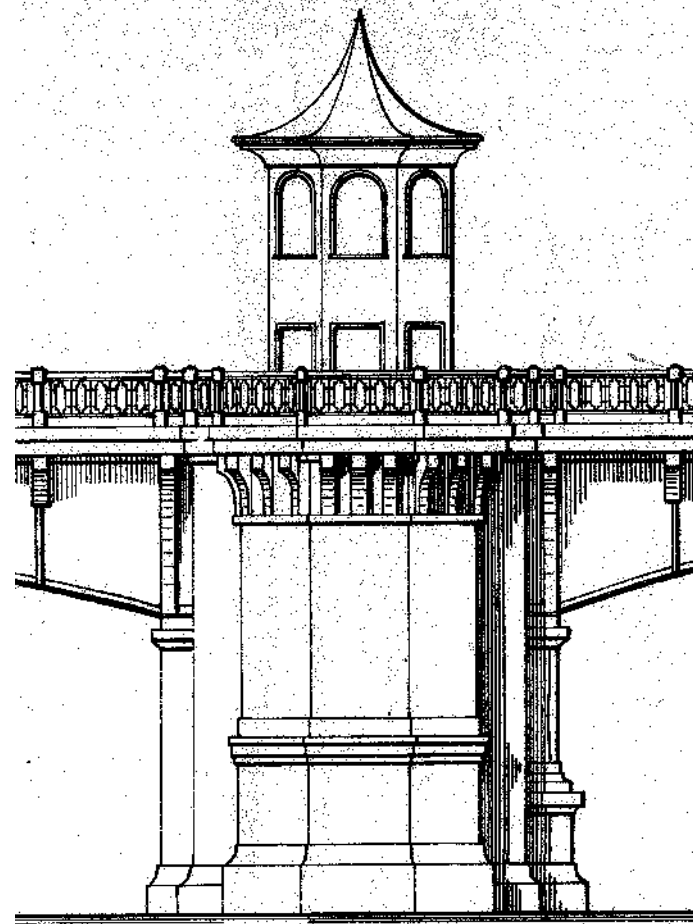
The 1924 design development plans reflect a more simplified design approach than the revised construction plans. Single curvilinear pathways lead to the bridge in the West Park Approach while the East park Approach is almost void of any design intent.

The revised construction plans reflect a stronger relationship with the surrounding context. For instance, the West Park Approach is made up of circles and radial pathways which relate directly to the layout of the Ponce de Leon Circle and Anderson Circle, adjacent to the city's historic square.

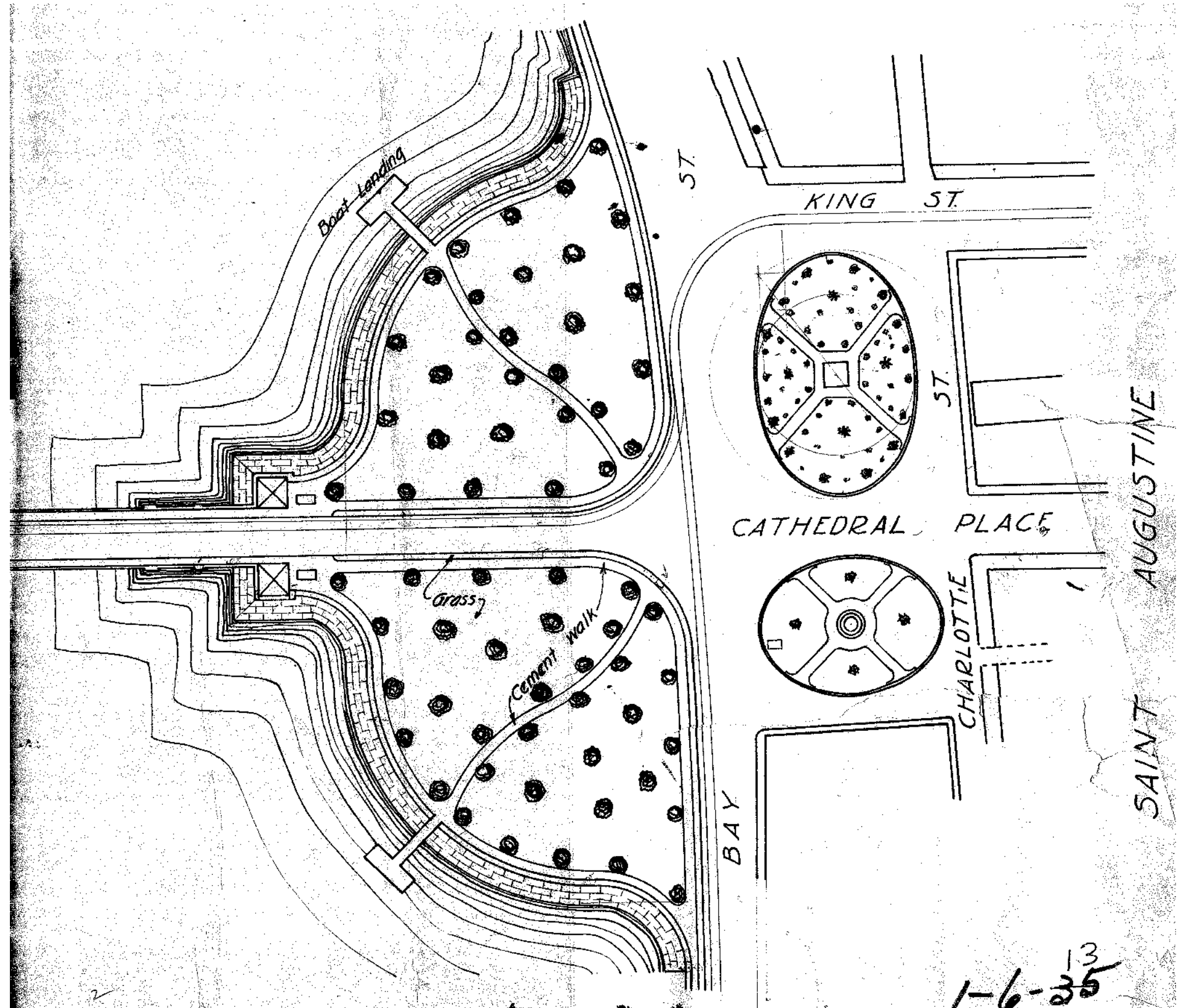
Some of the details, including the lamp posts and railings were incorporated into the bridge's actual construction. Details of the toll house and boat landing, on the other hand, were never installed during the actual construction phase.

WEST PARK APPROACH

1924 Design Development Plan



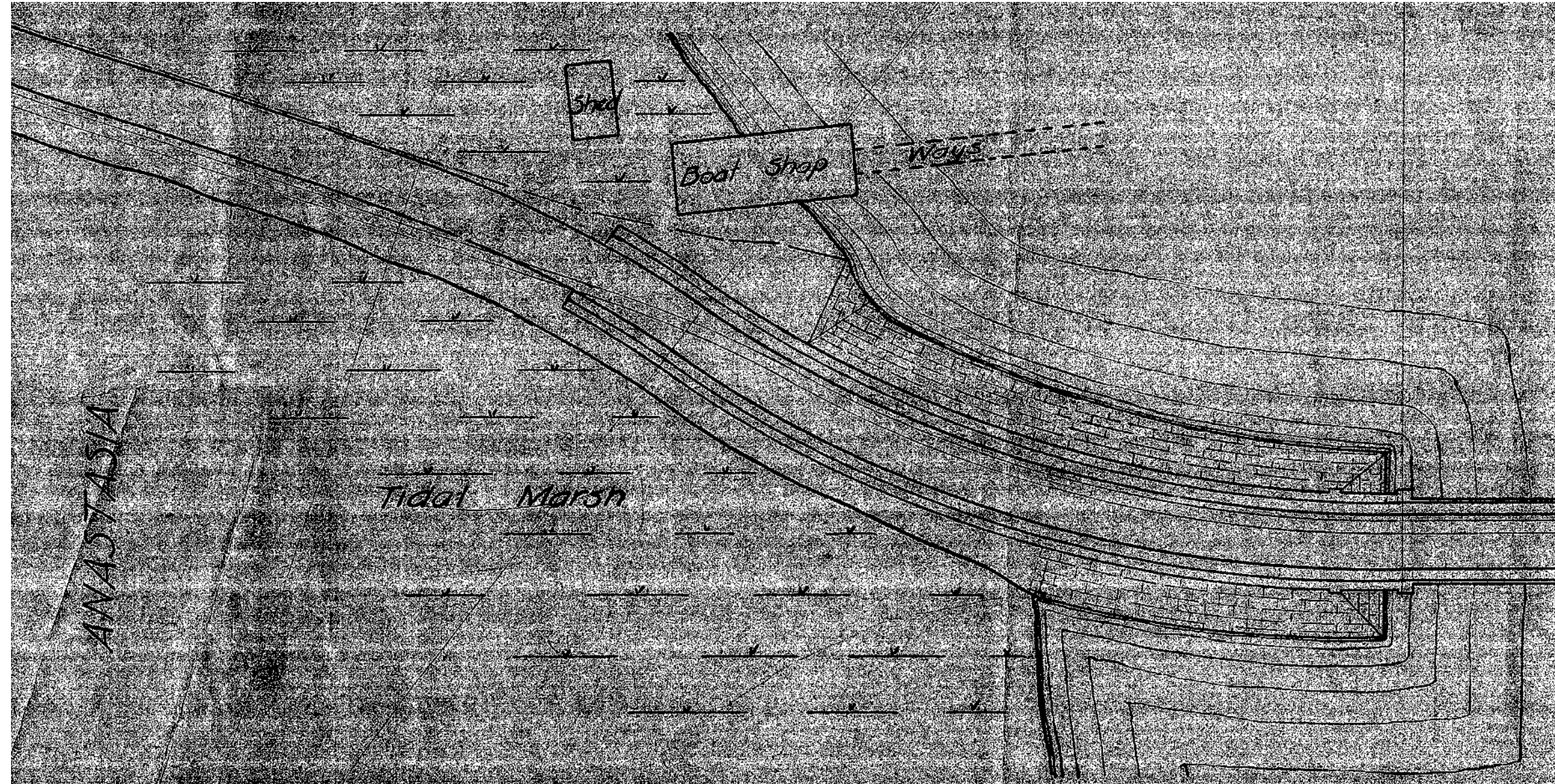
Proposed Bridge Bascule Design



1-6-35¹³

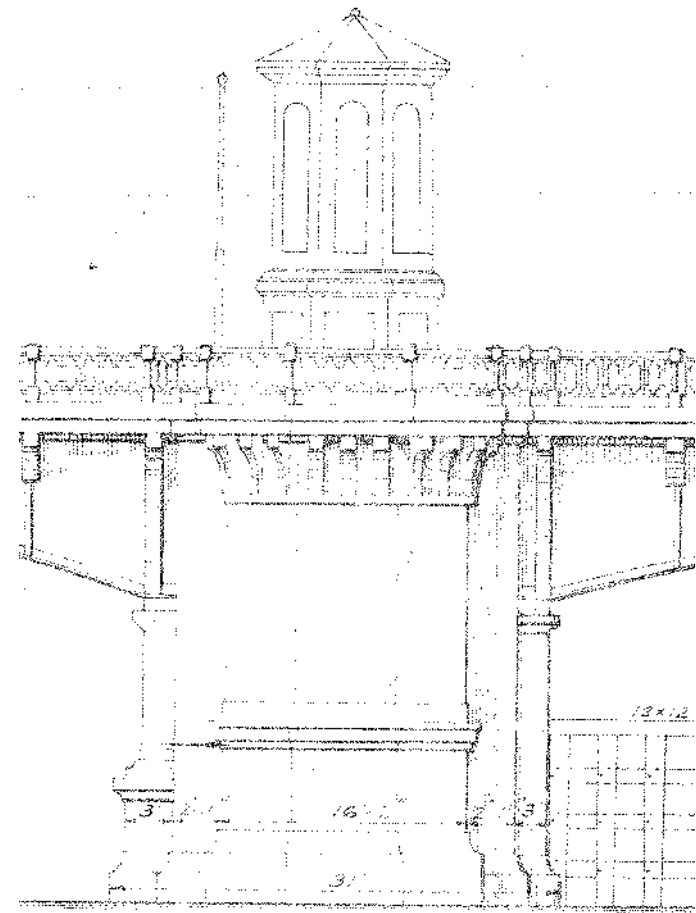
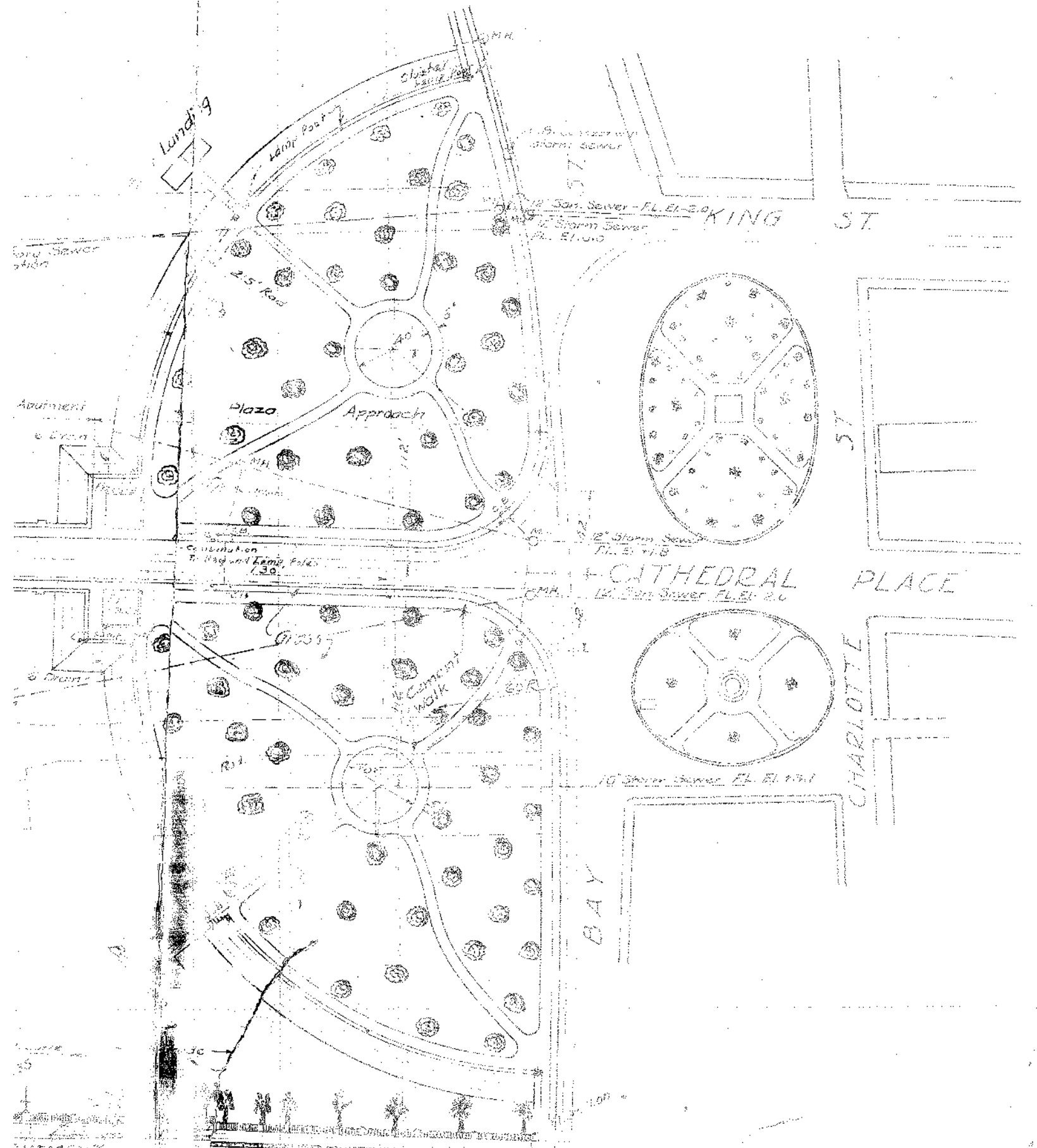
EAST PARK APPROACH

1924 Design Development Plan



WEST PARK APPROACH

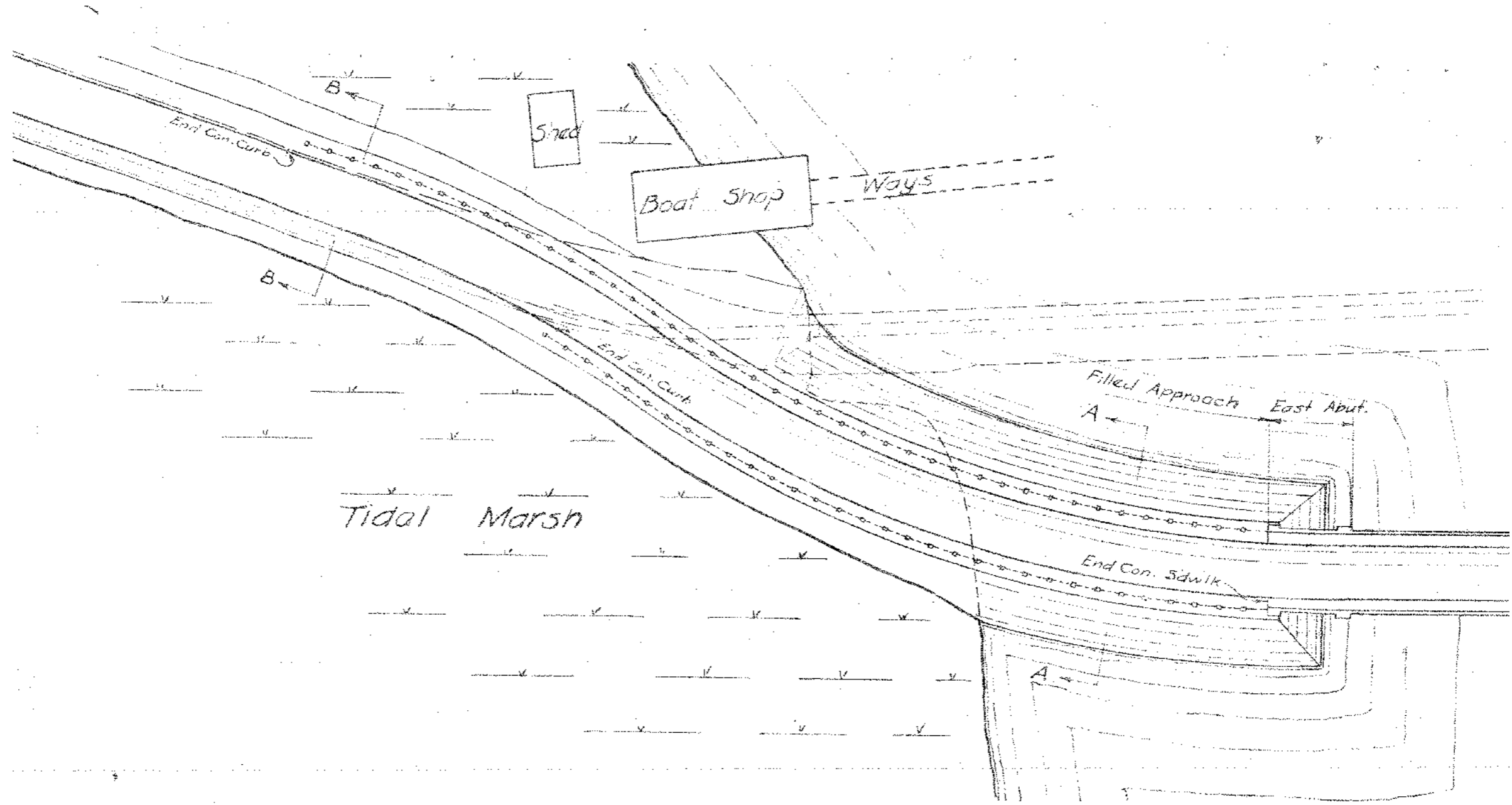
1924 Construction Plan



Adopted Bridge Bascule Design

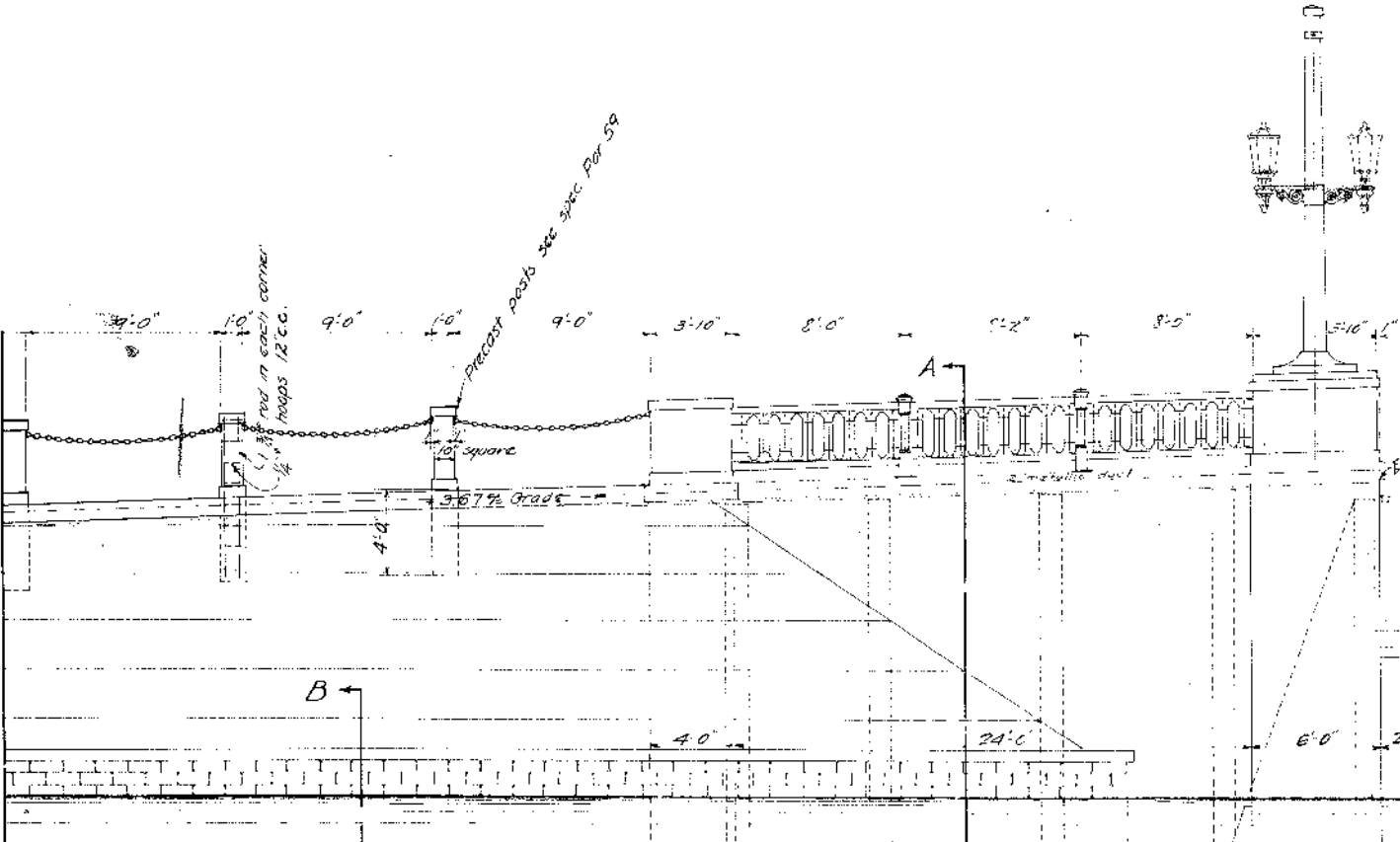
EAST PARK APPROACH

1924 Construction Plan

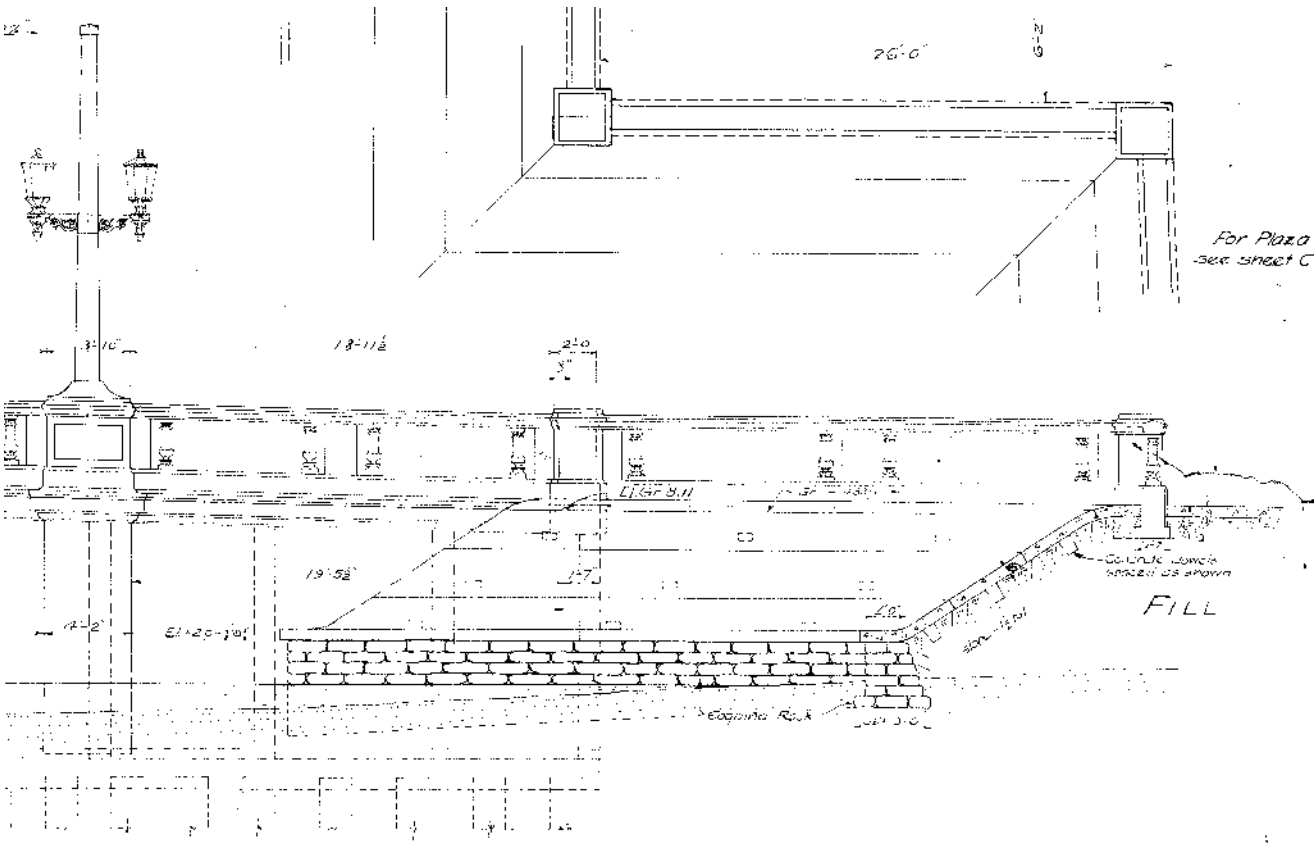


ELEVATIONS OF EAST AND WEST APPROACHES

1924 Construction Details



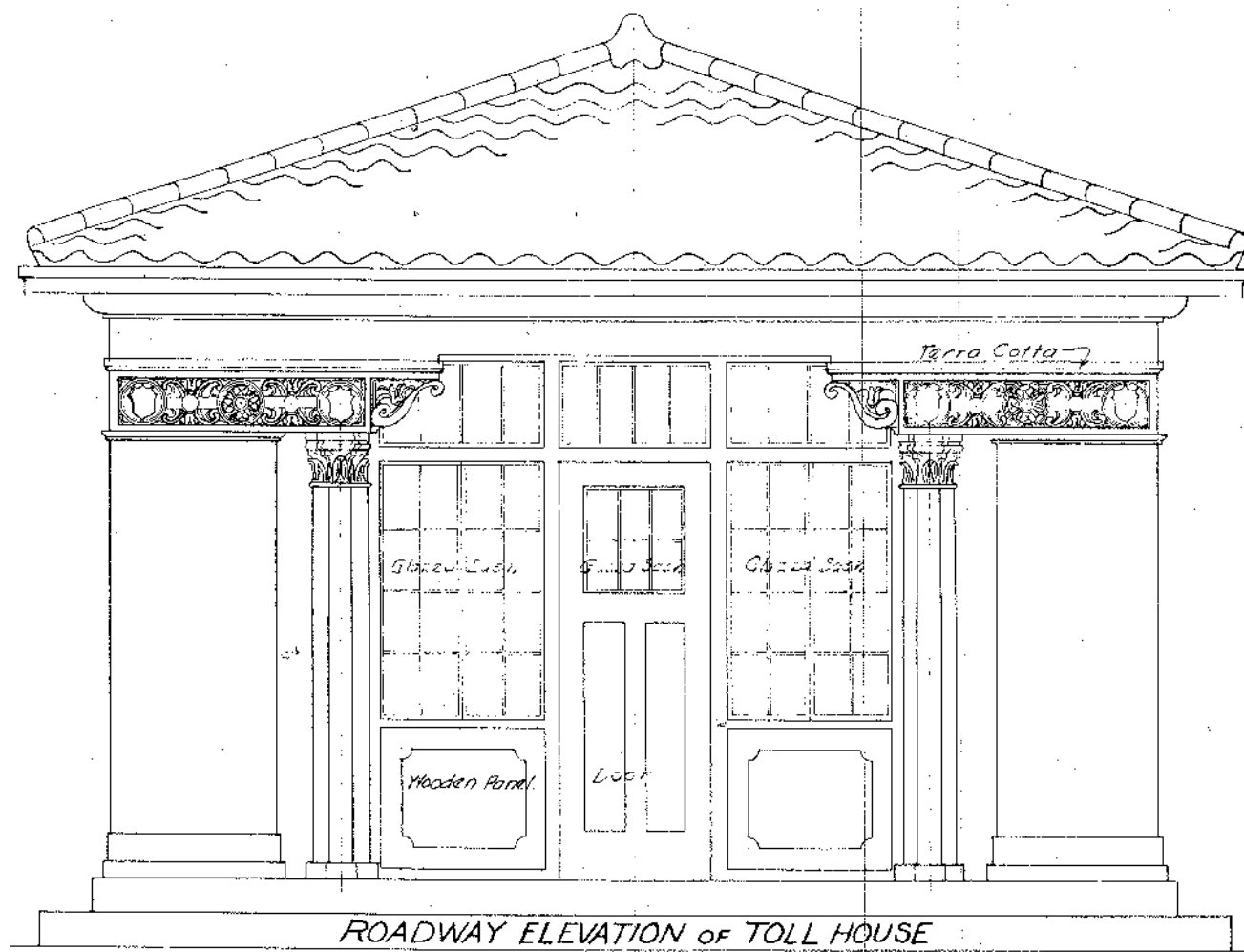
Elevation of East Approach



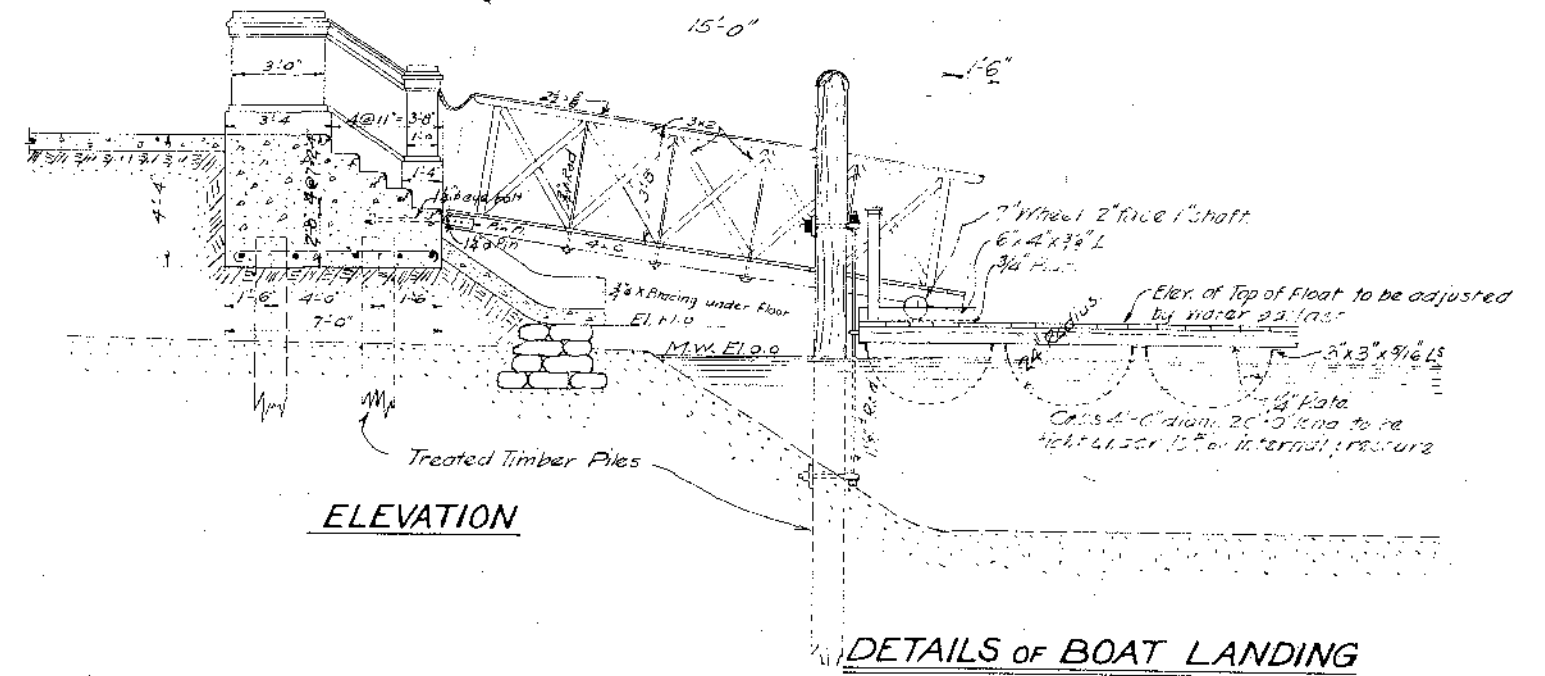
Elevation of West Approach

PROPOSED STRUCTURES

1924 Construction Details



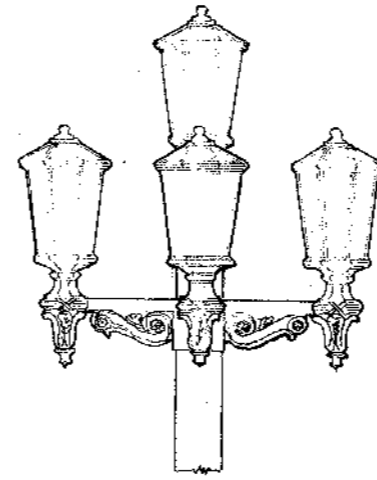
Elevation of Proposed Toll House



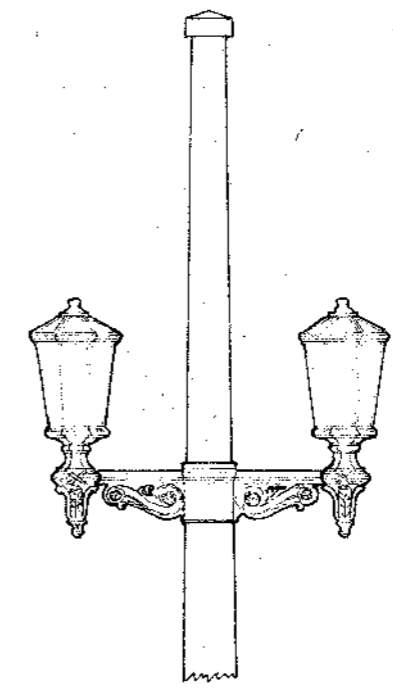
Elevation of Proposed Boat Landing

WEST PARK APPROACH

1924 Construction Details

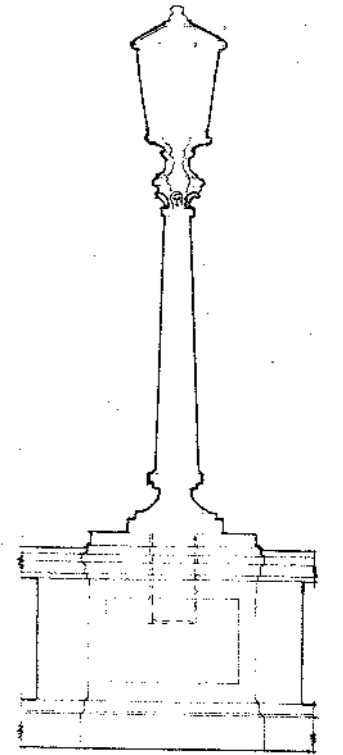


Cluster Lamp



Outside face of steel bars to be $\frac{3}{4}$ " from outside surface of pole.

Double Lamp

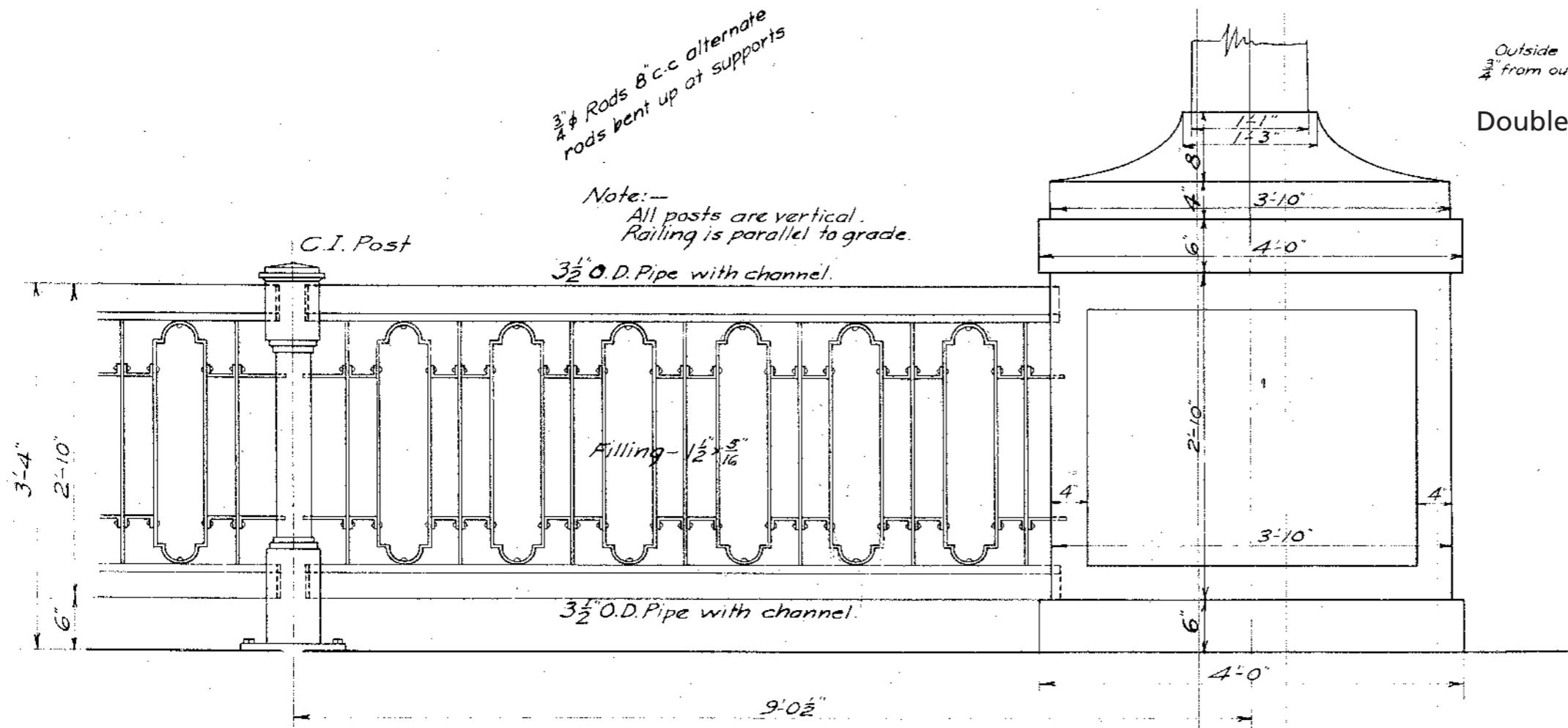


LIGHT POST SET IN BALUSTRADE POST ON PLAZA

Single Lamp

$\frac{3}{4}$ " Rods 8" c.c. alternate rods bent up at supports

Note:—
All posts are vertical.
Railing is parallel to grade.



DETAIL OF RAILING

Scale: -1"=1FT.

Elevation of Bridge Railing

Existing Conditions and Assessment of Park Approaches

Survey Procedure

This section of the report serves as a photographic compendium of existing conditions for the west and east approaches leading to the Bridge of Lions.

Information about the current condition of all site features and plant materials is separated into the two areas "West Park Approach" and "East Park Approach." Within each area, all items are summarized in an evaluation matrix, based on the following criteria:

- **Physical Condition**

1: poor, 2: fair, 3: good, 4: excellent

- **Age**

1: 1-20 years, 2: 20-40 years, 3: 40-60 years, 4: 60 or more years

- **Historic Appropriateness**

1: not appropriate, 2: fairly, 3: very, 4: extremely appropriate; based primarily on a feature's appropriateness to the 1927 period design intent, and secondarily on its use value or track record of success.

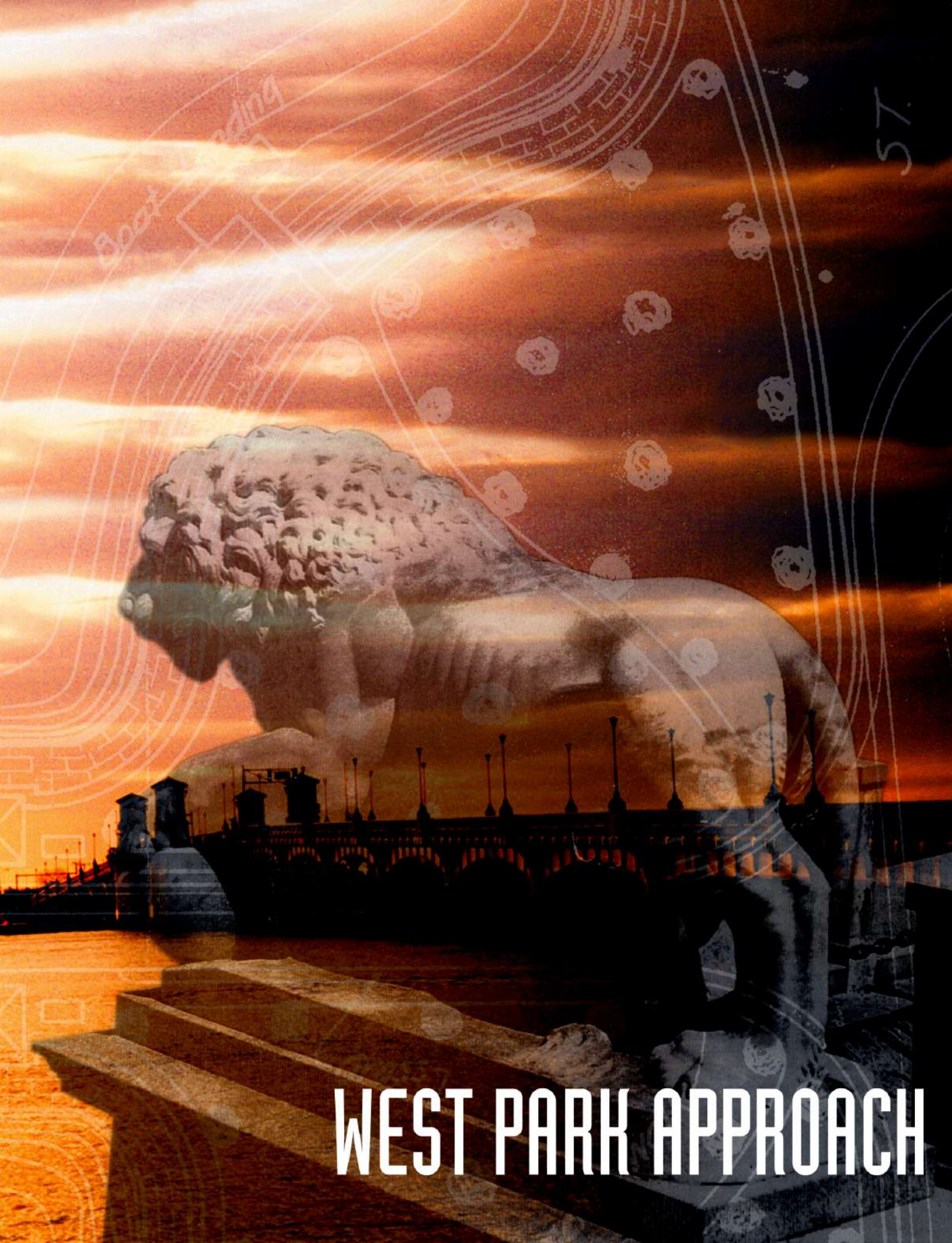
- **Quantity**

The quantity of each feature listed is given in number of items, lineal feet, or square feet

- **Description**

Each feature listed is described in terms of its material composition, or architectural style, including fasteners and other connection points

A site analysis map is included for both the "West Park Approach" and "East Park Approach" areas, depicting lines of jurisdiction/ownership and environmental/social features.



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WEST PARK APPROACH

WEST PARK SITE ANALYSIS

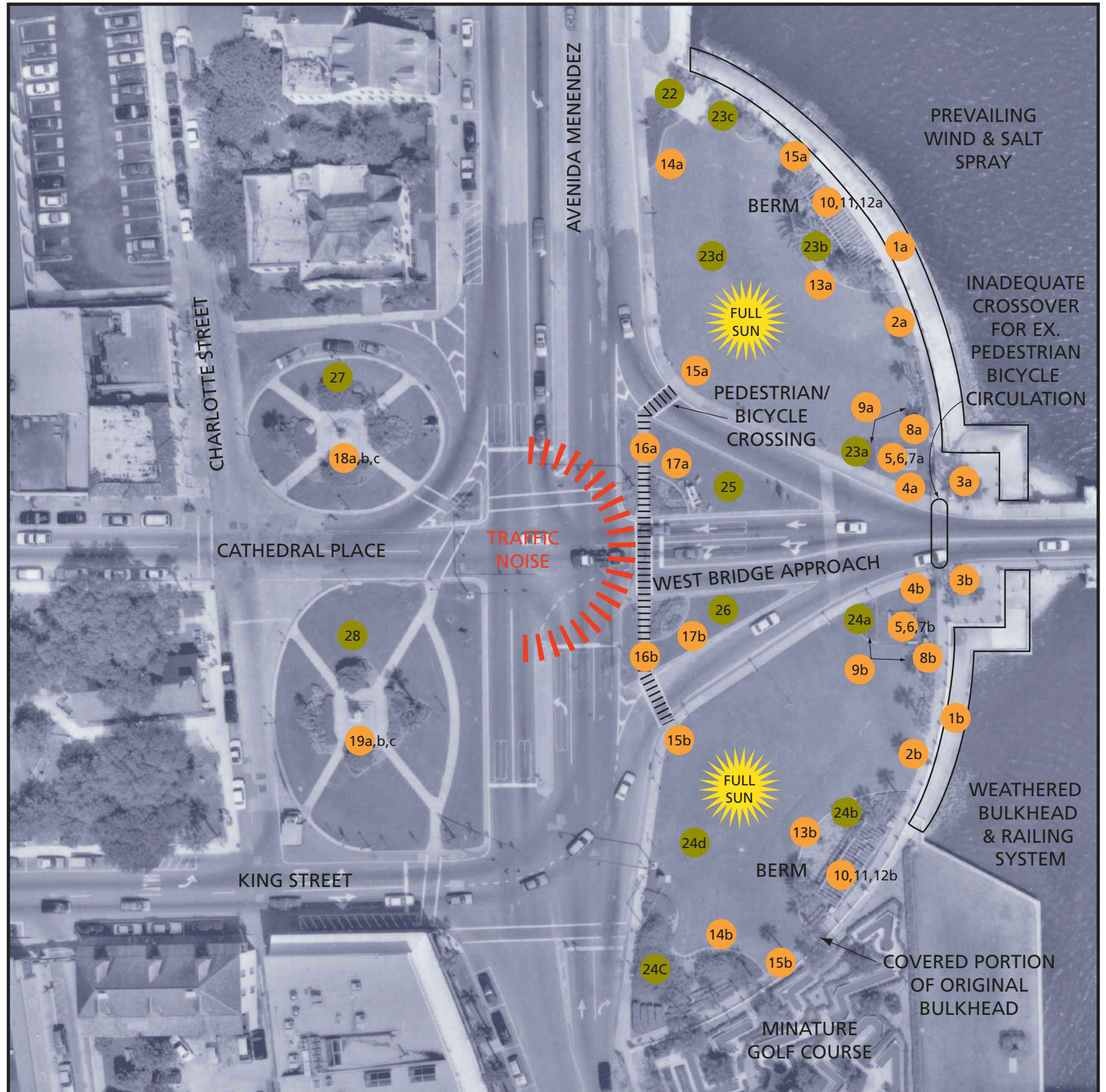
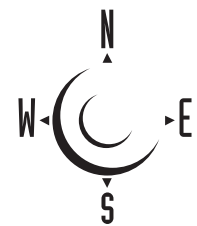
LEGEND

#	SITE FEATURES
1a	North Bulkhead
1b	South Bulkhead
2a	North Railing System
2b	South Railing System
3a	North Planter
3b	South Planter
4a	North Plaza
4b	South Plaza
5a	North Lion
5b	South Lion
6a	North Lion Base
6b	South Lion Base
7a	North Lion Planter
7b	South Lion Planter
8a	North Plaza Benches
8b	South Plaza Benches
9a	North Plaza Edging
9b	South Plaza Edging
10a	North Pergola
10b	South Pergola
11a	North Pergola Paving
11b	South Pergola Paving
12a	North Pergola Benches
12b	South Pergola Benches
13a	North Pergola Edging
13b	South Pergola Edging
14a	North Lower Bed Edging
14b	South Lower Bed Edging
15a	North Walkway
15b	South Walkway
16a	North Median Walk
16b	South Median Walk
17a	North Median Edging
17b	South Median Edging
18a	Flagpole
18b	Flagpole Base
18c	Flagpole Walkway
19a	Statue
19b	Statue Base
19c	Statue Walkway
20a	North Trash Cans (not shown)
20b	South Trash Cans (n.s.)
21a	North Gas Street Lamps (n.s.)
21b	South Gas Street Lamps (n.s.)
22	Park Dedication Marker (n.s.)

LANDSCAPE PLANTING

23	Northside of Approach
23a	North Upper Plaza
23b	North Pergola Planting
23c	North Lower Planting Area
23d	North Lawn
24	Southside of Approach
24a	South Upper Plaza
24b	South Pergola Planting
24c	South Lower Planting Area
24d	South Lawn
25	North Median
26	South Median
27	Anderson Circle
28	Ponce de Leon Circle

NOTE: Also refer to "Evaluation Matrix of West and East Park Approach" at the end of the "Existing Conditions" chapter



West Park Approach Existing Conditions

Approximately 75% of the west approach's open space has been left intact since the widening of Avenida Menendez into a four lane road during the late '50s. At that time, a portion of the approach was converted into two planted medians to separate the bridge's on and off ramps from the main intersection lanes. Because almost 25% of the park's area had to be removed to accommodate the widening, the two central circles and axial walkways of the original design were obliterated and replaced with an open lawn area. The street lamps of the original park approach design were also removed during this time of road widening and general bayfront "beautification" and replaced with late 1800s Philadelphia gas lamps.

In the mid-80's, another beautification effort was implemented resulting in the addition of two pergolas located at the edge of the north and south seawalls, re-location of the lion statuary 40' west of their original location, and expansion of existing landscape beds into larger, more curvilinear designs.

Site Analysis

Several major constraints at the West Park Approach include traffic noise, lack of windbreak protection from prevailing winds, minimal shelter from the sun, a high water table and periodic flooding with subsequent salt intrusion, and salt spray. These constraints create harsh conditions, adversely affecting both the physical comfort of pedestrians and condition of the park's site features and landscape planting.

Because the semi-circular walkway running along the edge of the seawall is setback at least 120' at its farthest point from the corner of the King St./Avenida Menendez intersection, there are landscape opportunities for prioritizing pedestrian access and seating at this location. The two pergolas, which had been installed in the park during the mid-80's beautification period, are currently located at these farthest points.

One of the major constraints of the west park is that the location of a pedestrian/bicycle pathway along the semi-circular edge of the bulkhead encourages unsafe circulation across the road near the bridge's entrance where there is currently no designated crosswalk. The only designated crosswalk is located on the western side of the park space at the intersection of Avenida Menendez. A design strategy for providing safer pedestrian/bicycle circulation is recommended for consideration during the design development phase of the bridge's rehabilitation effort.

The West Park Approach's most historically significant features are the original lion statues, now over 75 years in age. Although, consideration was given to repair the condition of the lions during their re-location in the mid-1980's, there is now evidence from current research that suggests the lions were not provided any conservation treatment during relocation. Only the lions' pedestals and bases were replaced to provide better structural integrity against the negative effects of soil erosion and settling. Because of the lions' historical value, developing a method for their proper conservation treatment will be given priority during the site's design development phase.

The two pergolas, which were installed in the mid-1980's, are in good condition. Because the pergolas were surrounded by planted berms at the time of their installation, they offer some protection from traffic noise and visual exposure to the street. The pergolas are not part of the original design, however, and their historic appropriateness should also be taken into account in the overall rehabilitation effort.

The majority of plant materials chosen for the West Park Approach appear to be in good condition in spite of their exposure to harsh environmental conditions. The sabal palm, yaupon holly, and India hawthorn are excellent choices because of their native tolerance of the conditions. The native coontie fern, though typically quite resilient, appears to be suffering at the northeast end of the park, probably due to its extreme exposure to full sun, automobile exhaust, wind, and especially salt intrusion. If re-located to a slightly more protected area, the coontie fern will probably flourish.

Plants that have shown obvious signs of freeze damage are the hibiscus and lantana. Fortunately, these plants have only been used as accents in the West Park Approach and have not been integrated as large masses into the landscape design.

The pittosporum and elaeagnus, however, appear to be in fair to poor condition because they are maintained at a height well below their potential height and spread. Currently, their height is maintained between 3 and 4 feet, even though they can reach well over 15' in height at maturity. It is recommended that these plants eventually be substituted with plants that are naturally more compact in size and more easily maintained.

West Park Approach Site Features



Weathered lion and base



Aging post and chain railing system along bulkhead



Anderson Circle



Ponce de Leon Circle



North pergola at west park approach



Cracked concrete base of flagpole



South pergola at west park approach



South portion of west park bulkhead



North portion of west park bulkhead



Planter curb at original location of lion statue



Aging bulkhead



N. lower bed edging



Railing system adjacent to miniature golf course



Dedication marker



Cracked and rusted railing system



North plaza benches and colored concrete paving



Corner post at north end of west park approach



Wood trash can



Pergola interior



North pergola edging

West Park Approach Landscape Planting



Planting at Anderson Circle



Planting at Ponce de Leon Circle



Freeze damage of Hibiscus at Anderson Circle



Declining Coontie Fern



Pittosporum at s. upper plaza



Thriving Coontie Fern



North lawn at west park approach



India Hawthorne at south pergola



S. lower planting area



Sable and Date palm at s. pergola

Evaluation Matrix

West Park Approach

Physical Condition (1-4)
 Age (1-4)
 Historic Appropriateness (1-4)
 Quantity

#	Site Feature					Description
1a	North Bulkhead	2	4	4	3,500 s.f.	concrete
1b	South Bulkhead	1	4	4	2,250 s.f.	"
2a	N. Railing System	1	3	4	325 l.f.	12" sq. bollards & chain
2b	S. Railing System	1	3	4	325 l.f.	"
3a	N. Planter	3	1	4	60 l.f.	6" sq. curb
3b	S. Planter	3	1	4	60 l.f.	"
4a	N. Plaza	3	1	2	900 s.f.	2' x 2' colored conc. pattern
4b	S. Plaza	3	1	2	900 s.f.	"
5a	N. Lion	3	4	4	1 ea.	marble replicas; small cracks
5b	S. Lion	3	4	4	"	"
6a	N. Lion Base	3	1	2	1 ea.	cast concrete
6b	S. Lion Base	3	1	2	"	"
7a	N. Lion Planter	2	1	2	54 l.f.	treated surface cracking off
7b	S. Lion Planter	2	1	2	"	"
8a	N. Plaza Benches	4	1	2	2 ea.	Granite, 60" x 18"
8b	S. Plaza Benches	4	1	2	"	"
9a	N. Plaza Edging	3	1	1	75 l.f.	8" concrete mow band
9b	S. Plaza Edging	3	1	1	"	"
10a	N. Pergola	4	1	1	400 s.f.	Wood uppers, stone base
10b	S. Pergola	4	1	1	"	"
11a	N. Pergola Paving	3	1	1	450 s.f.	2' x 2' concrete pattern
11b	S. Pergola Paving	3	1	1	450 s.f.	
12a	N. Pergola Benches	3	4	1	4 ea.	Granite, 60" x 18", 1 damaged
12b	S. Pergola Benches	4	4	1	"	Granite, 60" x 18"
13a	N. Pergola Edging	3	1	1	72 l.f.	8" concrete mow band
13b	S. Pergola Edging	3	1	1	"	"
14a	N. Lower Bed Edging	3	1	1	80 l.f.	8" concrete mow band
14b	S. Lower Bed Edging	3	1	1	24 l.f.	"
15a	N. Walkway	2	2	4	600 l.f.	Gray concrete
15b	S. Walkway	2	2	4	550 l.f.	"
16a	N. Median Walk	3	2	4	50 l.f.	Gray concrete
16b	S. Median Walk	3	2	4	40 l.f.	"
17a	N. Median Edging	3	1	1	33 l.f.	8" concrete mow band
17b	S. Median Edging	3	1	1	"	"
18a	Flagpole	3	4	4	1 ea.	Bronze & painted metal
18b	Flagpole Base	1	4	4	1 ea.	Concrete; chipped surface
18c	Flagpole Walkway	2	3	4	200 l.f.	Gray concrete
19a	Statue	3	4	4	1 ea.	Bronze
19b	Statue Base	2	4	4	1 ea.	Concrete
19c	Statue Walkway	2	3	4	380 l.f.	Gray concrete
20a	N. Trash Cans	4	1	1	3 ea.	wooden
20b	S. Trash Cans	4	1	1	3 ea.	"
21a	N. Gas Street Lamps	2	3	2	10 ea.	Rusted bases
21b	S. Gas Street Lamps	1	3	2	11 ea.	"
22	Park Dedicaton Marker	3	1	1	1 ea.	Granite with brass plate

• **Physical Condition**

- 1: poor
- 2: fair
- 3: good
- 4: excellent

• **Age**

- 1: 1-20 years
- 2: 20-40 years
- 3: 40-60 years,
- 4: 60 or more years

• **Historic**

Appropriateness

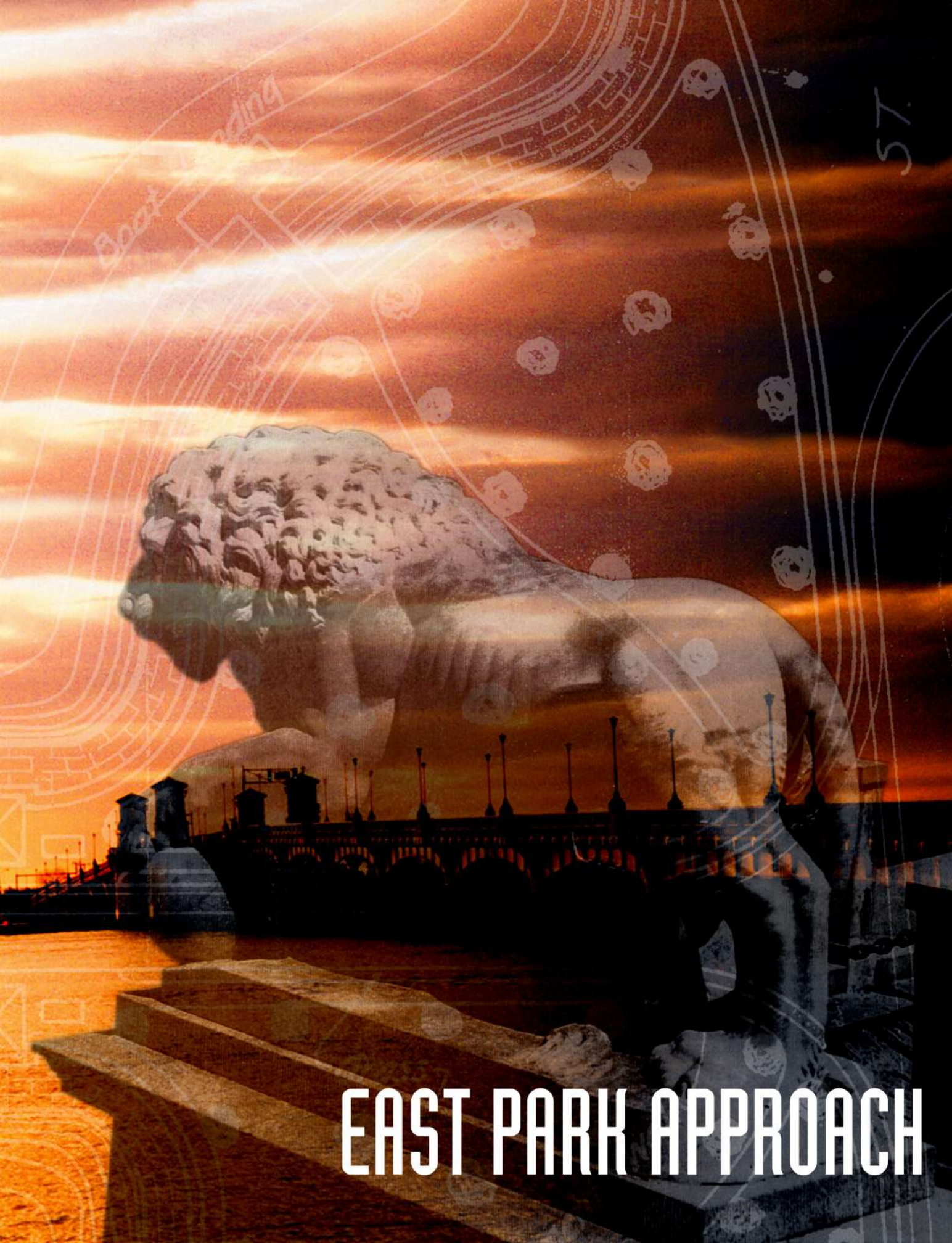
- 1: not appropriate
- 2: fairly,
- 3: very,
- 4: extremely appropriate

Physical
 Condition (1-4)
 Quantity

#	Plant Type*			Description
North Upper Plaza				
23a1	Sabal Palm	3	9 ea.	20-30' clear trunk
23a2	Pittosporum Hedge	1	40 s.f.	suffering from wind and salt spray
23a3	India Hawthorn Hedge	3	45 s.f.	"
23a4	Yaupon Holly	3	100 s.f.	"
23a5	Hibiscus	2	16 ea.	freeze damage
23a6	Coontie Fern	1	15 ea.	suffering from wind and salt spray
23a7	Muhly Grass	3	22 ea.	North Pergola Planting
23b1	Sabal Palm	3	6 ea.	20-30' clear trunk
23b2	Date Palm	3	3 ea.	varies
23b3	Pittosporum	2	240 s.f.	
23b4	India Hawthorn	3	300 s.f.	
23b5	Yaupon Holly	3	160 s.f.	
North Lower Planting Area				
23c1	Sabal Palm	3	4 ea.	20-30' clear trunk
23c2	Date Palm	2	3 ea.	
23c3	Pittosporum	3	36 s.f.	
23c4	India Hawthorn	2	25 s.f.	
23c5	Creeping Juniper	3	300 s.f.	
23c6	Iceplant	3	140 s.f.	
North Lower Lawn				
23d	St. Augustine Grass	3	17,500 s.f.	
South Upper Plaza				
24a1	Sabal Palm	3	9 ea.	20-30' clear trunk
24a2	Pittosporum Hedge	2	40 s.f.	
24a3	India Hawthorn Hedge	3	45 s.f.	
24a4	Yaupon Holly	3	100 s.f.	
24a5	Hibiscus	2	16 ea.	freeze damage
24a6	Coontie Fern	3	40 s.f.	
24a7	Muhly Grass	3	38 ea.	
South Pergola Planting				
24b1	Sabal Palm	3	6 ea.	20-30' clear trunk
24b2	Date Palm	3	3 ea.	varies
24b3	Pittosporum	2	240 s.f.	
24b4	India Hawthorn	3	300 s.f.	
24b5	Yaupon Holly	3	160 s.f.	
South Lower Planting Area				
24c1	Sabal Palm	2	4 ea.	20-30' clear trunk
24c2	India Hawthorn Tree	3	1 ea.	7' in height
24c3	Elaeagnus Hedge	2	200 s.f.	
24c4	India Hawthorn	3	180 s.f.	
24c5	Yaupon Holly	3	90 s.f.	
24c6	Cassia	3	9 ea.	deciduous
24c7	Hibiscus	2	3 ea.	freeze damage
24c8	Daylilies ea.	3	36 ea.	
24c9	Lantana	2	110 s.f.	freeze damage
South Lawn				
24d	St. Augustine Grass		17,500 s.f.	

Physical
 Condition (1-4)
 Quantity

#	Plant Type			Description
North Median				
25a	Annual Bed	3	225 s.f.	
25b	St. Augustine Grass	3	2,600 s.f.	
South Median				
26a	Annual Bed	3	225	
26b	St. Augustine Grass	3	2,600 s.f.	
Anderson Circle				
26a	Sabal Palm	3	3 ea.	20-30' clear trunk
26b	Magnolia	3	2 ea.	
26c	Burford Hedge	2	180 s.f.	
26d	Podocarpus Hedge	2	60 s.f.	
26e	India Hawthorn	2	30 s.f.	
26f	Hibiscus	2	40 s.f.	freeze damage
26g	Coontie Fern	2	16 ea.	
26h	Pampas Grass	3	9 ea.	
26i	Daylilies	3	20 ea.	
26j	Lantana	2	80 s.f.	freeze damage
26k	Annuals	2	80 s.f.	
26l	St. Augustine Grass	3	6,000 s.f.	
Ponce de Leon Circle				
28a	Sabal Palm	3	3 ea.	20-30' clear trunk
28b	Magnolia	3	2 ea.	
28c	Pittosporum Hedge	2	60 s.f.	
28d	Burford Hedge	2	60 s.f.	
28e	Podocarpus Hedge	3	60 s.f.	
28f	India Hawthorn	3	50 s.f.	
28g	Hibiscus	2	40 s.f.	freeze damage
28h	Coontie Fern	2	16 ea.	
28i	Pampas Grass	3	9 ea.	
28j	Lantana	2	60 s.f.	freeze damage
28k	Annuals	2	120 s.f.	
28l	St. Augustine Grass	3	12,000 s.f.	



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EAST PARK APPROACH

EAST PARK SITE ANALYSIS

LEGEND

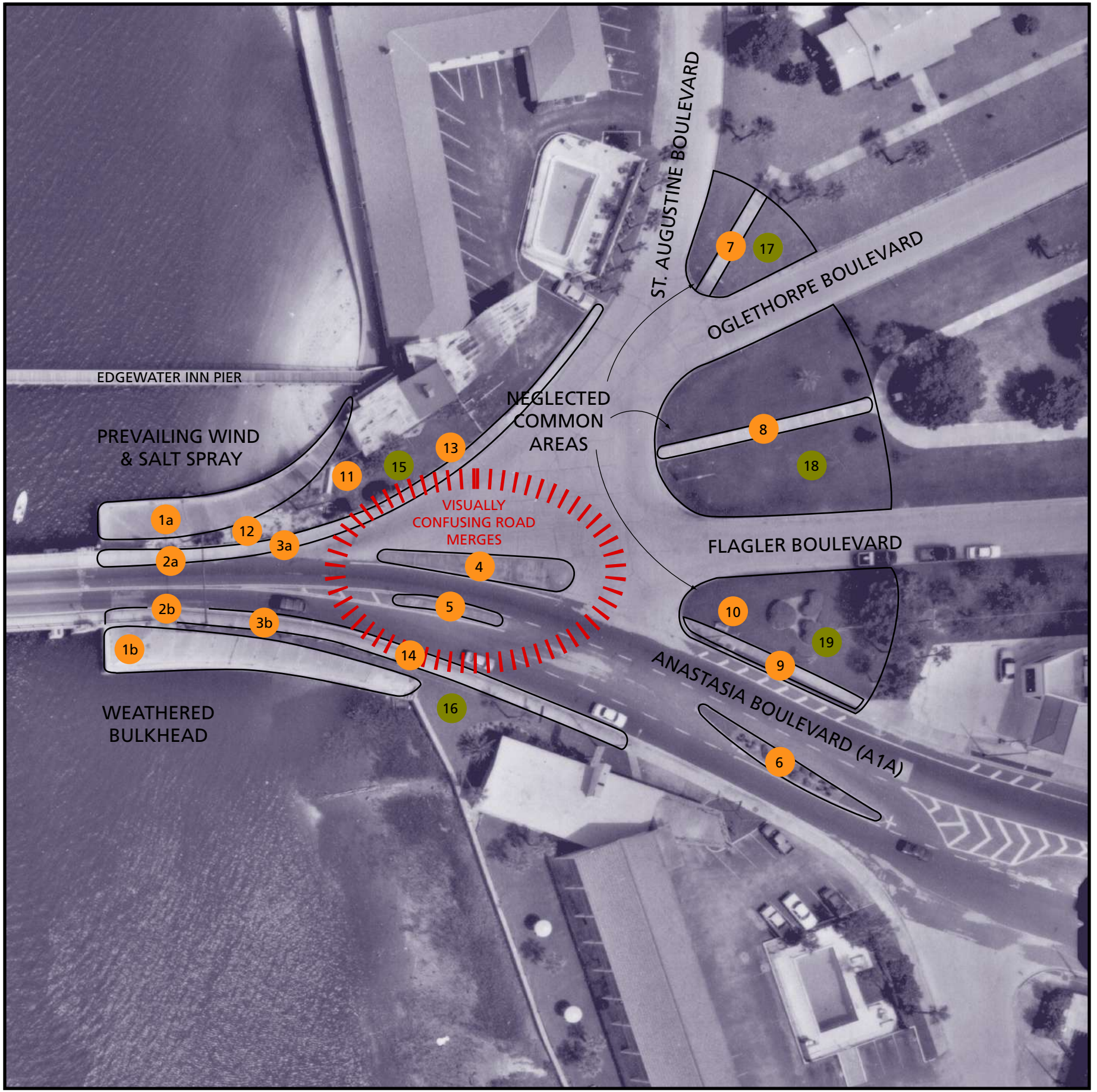
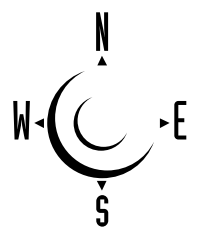
SITE FEATURES

- 1a North Bulkhead
- 1b South Bulkhead
- 2a North Railing System
- 2b South Railing System
- 3a North Sidewalk
- 3b South Sidewalk
- 4 North West Median
- 5 South West Median
- 6 East Median
- 7 North East Sidewalk
- 8 North Central Sidewalk
- 9 South East Sidewalk
- 10 Bench
- 11 North Edgewater Inn Sign
- 12 North Guard Rail
- 13 North East Wood Post & Chain
- 14 Sewer Manhole

LANDSCAPE PLANTING

- 15 North Planting Area
- 16 South Planting Area
- 17 Northeast Common Area
- 18 Central Common Area
- 19 Southeast Common Area

NOTE: Also refer to "Evaluation Matrix of West and East Park Approach" at the end of the "Existing Conditions" chapter



East Park Approach Existing Conditions

Although the East Park Approach is smaller in area and had never been designed to the same extent as the West Park Approach, it still provides a historic connection with the Davis Shores development and other attempts of the beautification movement of the early 1900's.

The East Park Approach still maintains the original axial system of roads leading into Davis Shores and down Anastasia Blvd. (A1A) from the bridge. Flagler Boulevard was originally designed to be on axis with the bridge as the prominent thoroughfare. However, small changes in road alignment and landscaping have occurred over the last fifty years giving priority to A1A, instead, as the main route for vehicular traffic across Anastasia Island.

Currently, the north and south sides of the bridge approach serve as small landscaped extensions of the private inns at those locations. Where the axial roads intersect, just before the bridge, small concrete medians have been located to direct merging traffic onto A1A from Davis Shores. Inside the angles created between each of the intersecting roads are small common areas, comprised of axial sidewalks, grass, and minimal landscaping.

Site Analysis

The East Park Approach is impacted with many of the same environmental conditions affecting the West Park Approach, including traffic noise and pollution, prevailing winds, salt spray and full sun conditions. However, the inn buildings located at the north and south sides serve as a windbreak, providing some protection of the plantings.

The original pre-cast concrete post and chain detailing is still located on either side of the bridge's approach, though in extremely poor condition. No other site features, besides the post and chain system, were developed in the original design of the east approach.

The major constraint of the East Park Approach is the lack of visual hierarchy between merging roads, necessary to properly orient drivers and provide them with a sense of direction. Several auto accidents have occurred, over the last several decades where drivers, traveling west from A1A to the bridge, have not made the bend and swerved northwest off the road and over the bulkhead. As a result, a guardrail was located behind the post and chain railing system to prevent further accidents. Attention should be given to a new road layout that does not jeopardize the safety of drivers, pedestrians, and bicyclists.

The landscape plantings adjacent to the inns appear to be fairly well maintained. The common areas, on the other hand, appear to overlooked as "leftover" spaces and, therefore, receive little or no maintenance. Opportunities exist for these common areas to be improved as public spaces and/or to provide land that may be needed to improve the existing road layout.

St. Augustine

The Bridge of Lions

Section 1: Data Gathering & Site Analysis of Park Approaches

East Park Approach Site Features and Landscape Planting



Concrete median at east park approach



Aging concrete post and chain railing system



Railing system in front of Edgewater Inn



North guardrail

East Park Approach Site Features and Landscape Planting



Pier and bulkhead at Edgewater Inn



South portion of bulkhead at east park approach



Bench at s.e. common area of east park approach

Evaluation Matrix

East Park Approach

Physical Condition (1-4)
Age (1-4)
Historic Appropriateness (1-4)
Quantity

#	Site Feature					Description
1a	North Bulkhead	2	4	4	1,300 s.f.	concrete
1b	South Bulkhead	1	4	4	1,800 s.f.	"
2a	N. Railing System	1	4	4	130 l.f.	12" cast concrete bollards and chain
2b	S. Railing System	1	4	4	130 l.f.	"
3a	N. Sidewalk	3	1	4	250 l.f.	gray concrete
3b	S. Sidewalk	2	1	4	250 l.f.	"
4	N.W. Median	2	2	1	825 s.f.	"
5	S.W. Median	2	2	1	275 s.f.	"
6	East Median	2	2	1	800 s.f.	"
7	N.E. Sidewalk	2	2	3	60 l.f.	"
8	N.C. Sidewalk	3	1	3	100 l.f.	"
9	S.E. Sidewalk	2	2	3	85 l.f.	"
10	Bench	3	3	2	1 ea.	Concrete and wood
11	North Edgewater Inn Sign	3	1	1	1 ea.	
12	N. Guardrail	3	1	1	95 l.f.	to reinforce barrier
13	N.E. Wood Post and Chain	3	1	1	60 l.f.	
14	Sewer Manhole	2	2	2	1 ea.	

- **Physical Condition**
 - 1: poor
 - 2: fair
 - 3: good
 - 4: excellent
- **Age**
 - 1: 1-20 years
 - 2: 20-40 years
 - 3: 40-60 years,
 - 4: 60 or more years
- **Historic Appropriateness**
 - 1: not appropriate
 - 2: fairly,
 - 3: very,
 - 4: extremely appropriate

Physical
Condition

Quantity

#	Plant Type			Description
North Planting Area				
15a	Sabal Palm	3	3 ea.	15-20' in height
15b	Date Palm	3	1 ea.	2' clear trunk
15c	India Hawthorn	3	125 s.f.	
15d	Philodendron	2	11 ea.	freeze damage
15e	Coontie Fern	3	3 ea.	large masses; can be divided
15f	Pampas Grass	3	12 ea.	
15g	Liriope 'Ev. Giant'	2	30 s.f.	
15h	St. Augustine Sod	2	2,250 s.f.	
South Planting Area				
16a	Date Palm	3	1 ea.	3' clear trunk
16b	Ligustrum Tree	3	3 ea.	7' in height
16c	India Hawthorn	3	25 s.f.	
16d	Boxwood	1	9 ea.	
16e	African Iris	3	50 s.f.	
16f	Aztec Grass	2	23 ea.	
16g	St. Augustine Sod	2	1,200 s.f.	
Northeast Common Area				
17	Lawn	1	2,200 s.f.	bare soil, patchy weeds
Central Common Area				
18	Lawn	1	9,000 s.f.	bare soil, patchy weeds
Southeast Common Area				
19a	Cedar	2	4 ea.	15-20' in height
19b	Sabal Palm	2	1 ea.	10' clear trunk
19c	Elaeagnus	1	180 s.f.	
19d	Lawn	2	3,850 s.f.	bare soil, patchy weeds

Review of Historical References

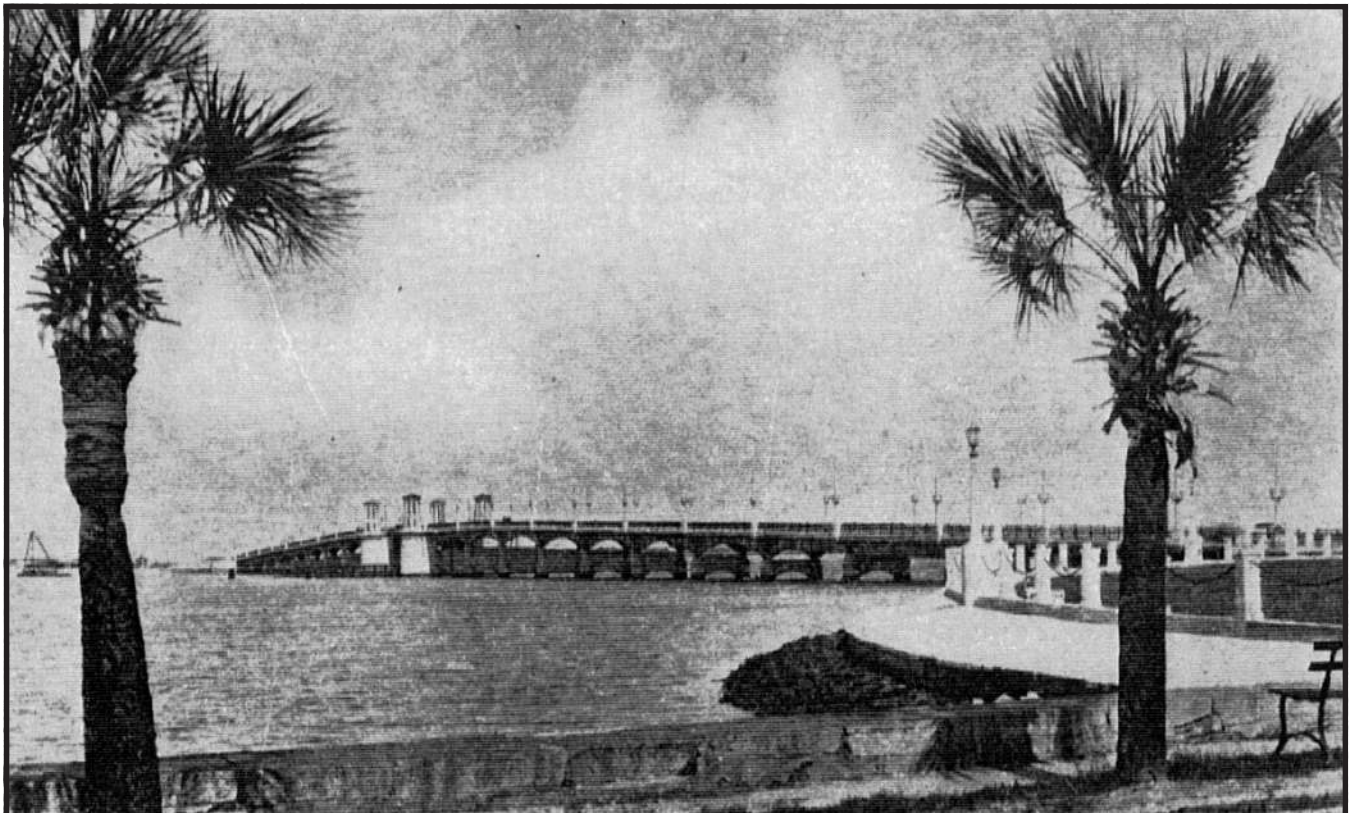
The New Plaza de Menendez

The west park space was nearly complete by the time the \$1,000,000 bridge was finished in early 1927. On April 4, 1927, the Acting Mayor, J.H. Manucy, made the following announcement in the St. Augustine Evening Record:

"At yesterday's meeting of the city commission, the matter of naming the beautiful new park space at the approach to the Matanzas River bridge was taken up, and after due consideration, it was the pleasure of the commissioners to pass a resolution creating a new municipal park area to be named in honor of the founder of the city, Pedro Menendez de Aviles.

The new park comprising an area of approximately two and a half acres is one of the most charming spots in the city and will be made more so, as grass, shrubbery and flowers are planted. The commissioners, in conferring the official title of Plaza de Menendez on this area, also stipulated that the territory be dedicated and named in connecton with the formal opening of the bridge on Thursday.

This newly created park area is immediately east of and in perfect harmony with the ancient Spanish Plaza de la Constitution, Anderson Circle, and Ponce de Leon Circle."



A 1928 postcard titled "New \$1,000,000 Bridge of Lions Connecting St. Augustine to Anastasia Island."



The official opening, 1927



Postcard of "Bridge of Lions Across Matanzas Bay..." 1927



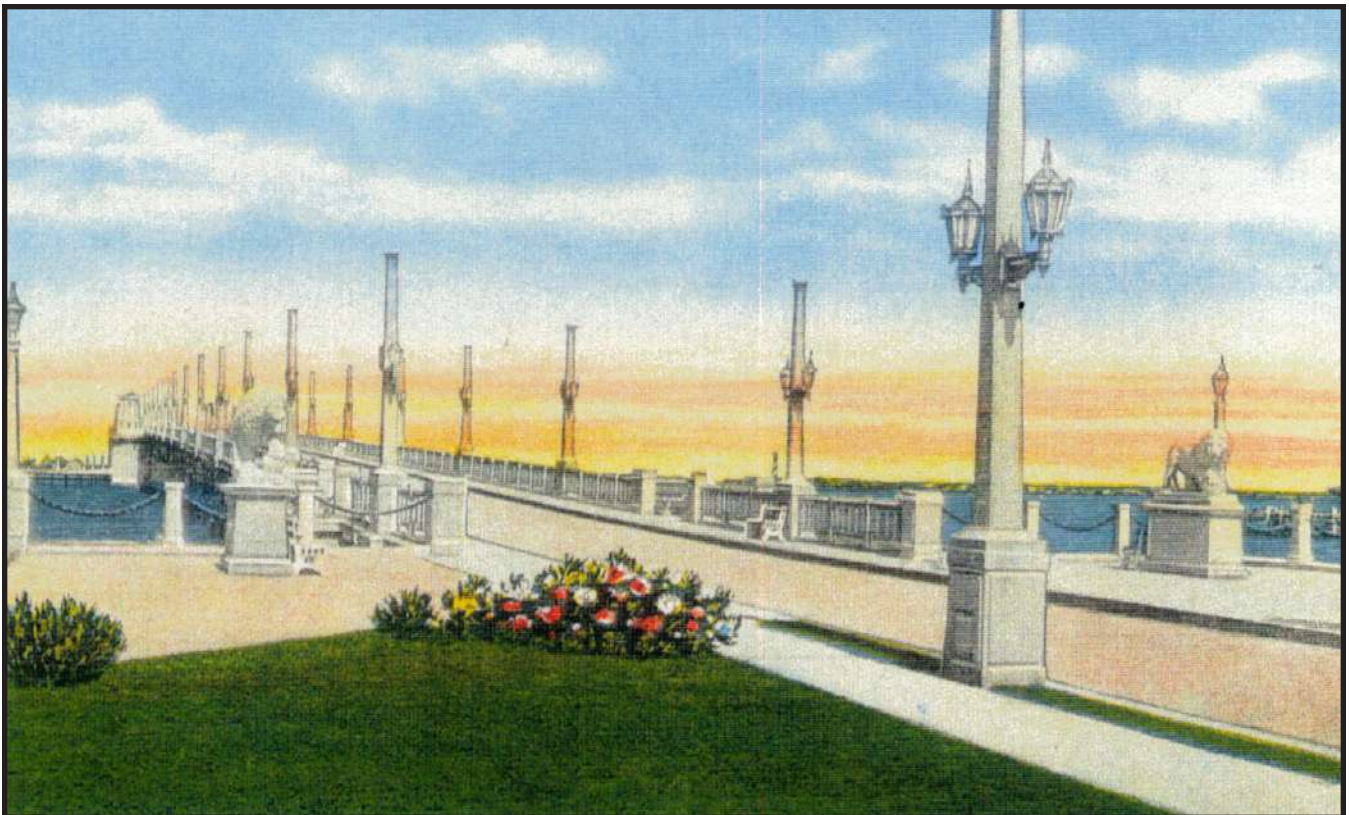
South end of west park approach, undated



North end of west park approach, undated



Completed bridge, facing downtown St. Augustine, March, 1927



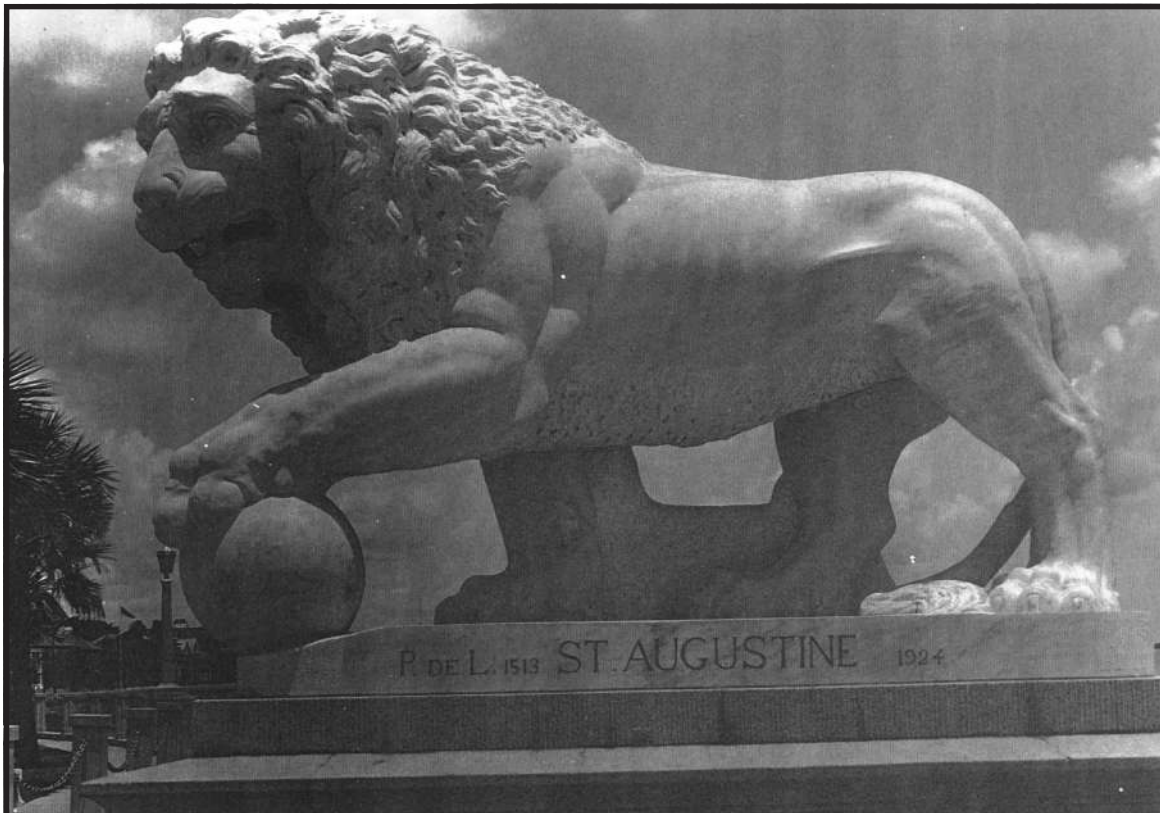
Postcard of the new Bridge of Lions, 1927

The Lions' Arrival

By the beginning of April 1927, the lion statuary had been placed on pedestals, following their importation by the estate of Dr. Andrew Anderson, who had died shortly after their acquisition. The following is a description of the lions' installation from the St. Augustine Evening Record on March 31, 1927:

"The lions, a decorative feature of the new span, are the gift of the late Dr. Andrew Anderson who, before his death, provided for their acquisition. At considerable cost, they have been imported by his estate from the studios of Romanelli in Florence, Italy, where they were fashioned in marble as enlarged replicas of two very famous statues in the Loggia di Lanzi.

Weighing more than 5,000 pounds each, the stone images will be located on pedestals four feet high on either side of the west approach to the bridge. The figures themselves, six feet long, standing five and a half feet above the pedestals, will face the city."



One of the marble lions, undated

Davis Shores and a Beautification Plan for the East Park Approach

Even though, the east park approach was far more simplified in its design than the west side approach, several plans were under way to develop entry features along the eastern approach.

D. P. Davis, developer of Davis Island in Tampa at the height of Florida's boom time, came to St. Augustine in order to create a similar resort island community on Anastasia Island. The following quote appears in a Davis Shores advertisement for the St. Augustine Record on Feb. 27, 2001:

"Now Davis Shores is truly within walking distance of down town St. Augustine, for over the new bridge there is no danger of a pedestrian being run down by a careless motorist, and there is a smooth surface for the feet instead of the rough and uneven planking of the old bridge..."

...Don't fail to drive or walk over the new bridge today and see Davis Shores from the draw span. The view is well worth the trip. Then go over the new avenues and boulevards of Davis Shores and consider the vast amount of work that has been done in bringing to existence here, on what was a marsh long after the bridge was started, an island city in the very heart of America's oldest community.

Take note of the beautification program that is now being worked out on Davis Shores, with long vistas of large palm trees, live oaks, oleanders, bamboos and flowering shrubs. Inspect the green parkways. Stop and examine the fine new modern residences and apartments on Davis Shores..."



Axial view into Davis Shores Development, March 5, 1927

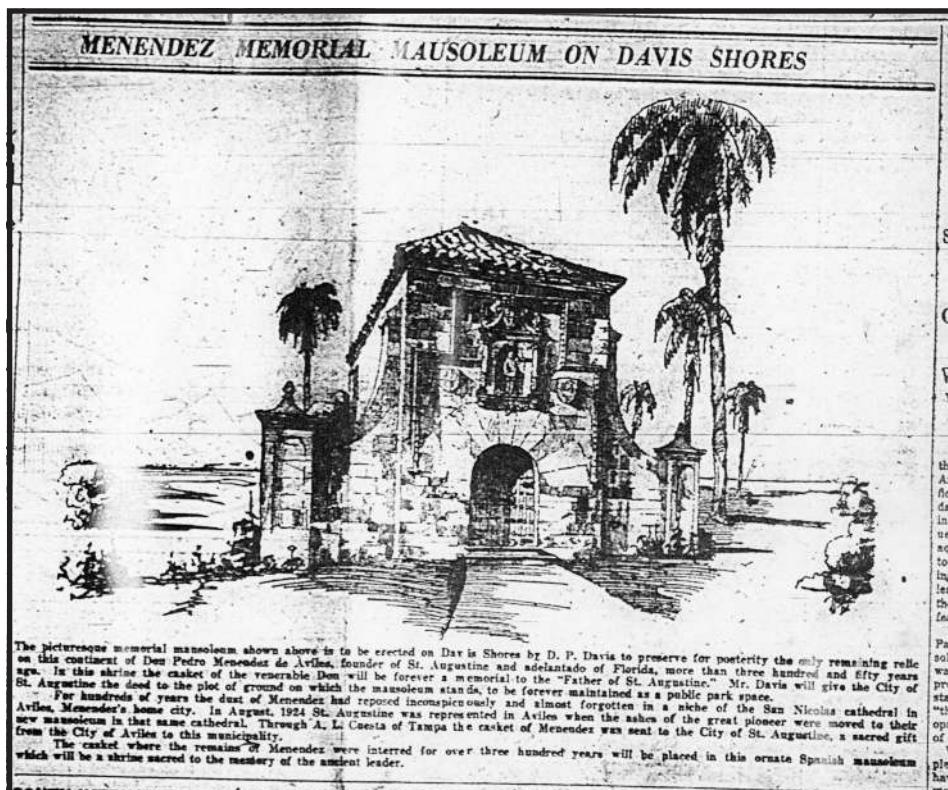
Another scheme by Davis was to create a site for the casket of St. Augustine's founder Don Pedro Menendez. Below is the caption of an illustration presented on February 14, 1926 in the St. Augustine Evening Record:

"The picturesque memorial mausoleum shown above is to be erected on Davis Shores by D.P. Davis to preserve for posterity the only remaining relic on this continent of Don Pedro Menendez de Aviles, founder of St. Augustine and adelantado of Florida, more than three hundred and fifty years ago. In this shrine the casket of the venerable Don will be forever a memorial to the "Father of St. Augustine." Mr. Davis will give the City of St. Augustine the deed to the plot of ground on which the mausoleum stands to be forever maintained as a public park space.

For hundreds of years the dust of Menendez had reposed inconspicuously and almost forgotten in a niche of the San Nicolas cathedral in Aviles, Menendez's home city. In August, 1924 St. Augustine was represented in Aviles when the ashes of the great pioneer were moved to their new mausoleum in that same cathedral. Through A.L. Cuesta of Tampa the casket of Menendez was sent to the City of St. Augustine, a sacred gift from the City of Aviles to this municipality.

The casket where the remains of Menendez were interred for over three hundred years will be placed in this ornate Spanish mausoleum which will be a shrine sacred to the memory of the ancient leader."

Unfortunately, the Great Hurricane of 1926 caused many investors to pull out of the Florida land market and Davis, like other Florida developers, left town before implementing most of his ideas. As a result, his visions for the Davis Shores Resort and the bridge's east park approach were never fully realized. The only noticeable relic of the Davis Shores master plan, that is immediately noticeable when crossing the bridge onto Anastasia Island, is the straight axial view from the bridge leading down Flagler Boulevard into the Davis Shores community, just before A1A bends to the right to continue south.



Plans for the Menendez Memorial Mausoleum

Depression Era and War Years

One major change to the West Park Approach that occurred during the 1940's was the backfilling of land around the bridge's south bulkhead to accommodate a new marina and recreational field. A smaller modification of the West Park's layout was the widening of the bridge's entry road to add a split lane for off-ramping traffic.

Development of Davis Shores and the bridge's East Park Approach continued slowly and sporadically throughout the 30's and 40's.



Avenida Menendez in the 1930's



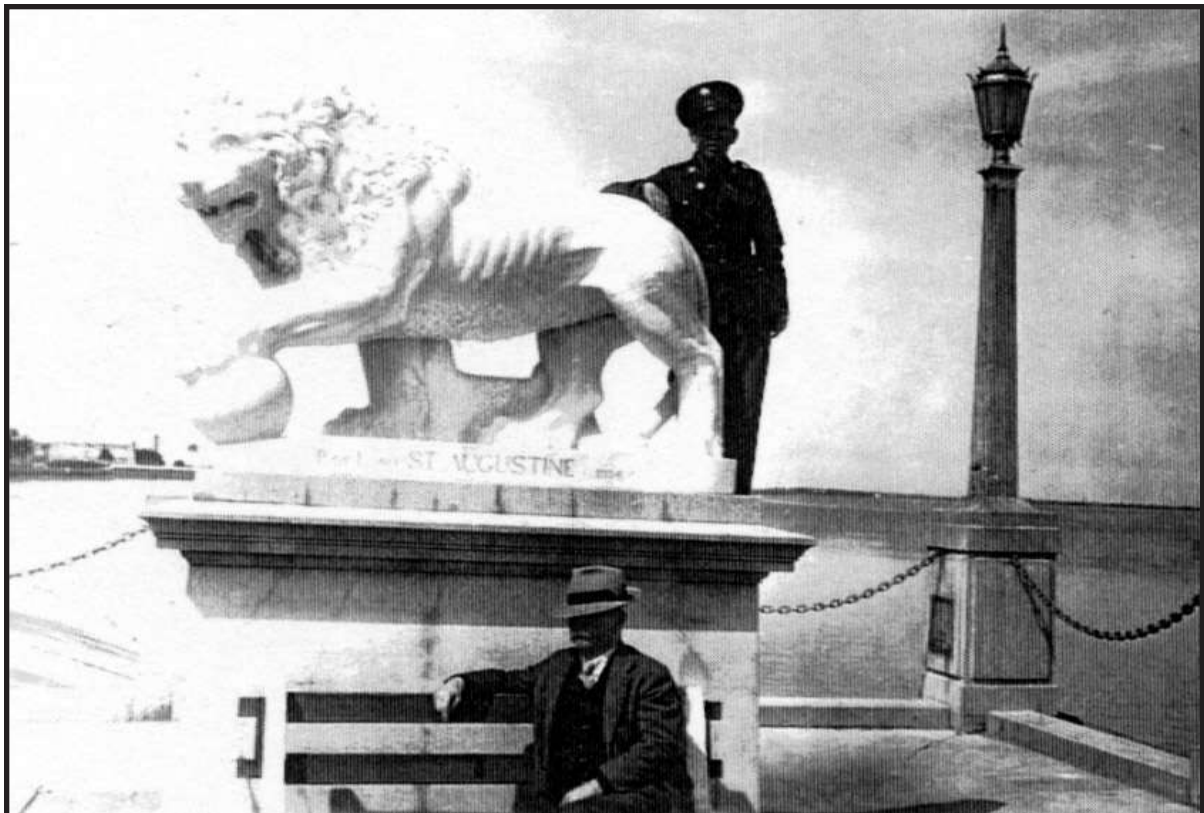
West park approach original 20's design layout



Workers on park grounds



Park grounds at west approach, undated



Men posing at lion, March 30, 1941



Aerial view of downtown St. Augustine, distant view of Davis Shores undeveloped, 1930's



Beginning of Davis Shores development, 1930's



J. Carver aerial of St. Augustine and Bridge, 1940's



Aerial 1940's



West park approach and marina, view facing west, 1940's



West park approach and marina, view facing north, 1940's



Aerial view 1940's, addition of split lane



Color postcard of Avenida Menedez prior to road widening, early 1950's

Post-War Boom and Expansion of Avenida Menendez

Since the mid 40's more attention was being given to the need for expanding road widths to accommodate local population growth and increased tourist traffic in and around St. Augustine. By the mid '50s, attempts to widen Avenida Menendez from a two to a four lane road created substantial controversy.

Appearing in The Florida Times on January 22, 1959 was the following announcement:

"Work started this week on construction of a new concrete seawall along St. Augustine's Bay Street to replace the coquina rock wall that was built by Army Engineers in the 1840's. Pilings for the new wall, are located at the edge of a fill about 80 feet from the old seawall. The fill will make room for a four-lane road, additional parking space and walkways."



The west park approach prior to road widening, view facing west, early 1950's



Old seawall, 1957-9



Expansion fill, 1957-9



Expansion fill, 1957-9



Prior to road widening, 1957-9



Breaking up of old seawall, 1957-9



Workers on old seawall, 1957-9



View east toward construction, 1957-9



Removing old seawall, 1957-9



View south toward construction, 1957-9



New seawall, 1957-9



Paving the new esplanade, 1957-9



Paving esplanade, 1957-9



Paving esplanade, 1957-9



End of new seawall, 1957-9



Road expansion at west park approach, 1957-9

While the sea wall was under reconstruction, Bay Street was being widened into a four lane road. In order to accommodate ramping on and off the bridge, a percentage of the original park's area was reduced to smaller median strip plantings. Once road construction was complete, a Bayfront beautification program was soon underway and included the re-planting of the Bridge's west park approach. Below was an article appearing in the St. Augustine Record on October 11, 1959:

"Beautification of the triangular planting beds at the west approach to the Bridge of Lions has been complete and work is now underway on preparing the Bay Street median strip for beautification.

...The triangles, planted with low shrubs and palm trees and sprigged with St. Augustine grass are shown in this view...

Meantime, work on the raised esplanada, or walkway, along the waterfront on Bay Street is proceeding at a fast rate."

The Bay Street beautification plan also included the purchase and installation of one hundred gas street lamps from Philadelphia to be placed along both the bayfront and west park approach



Article on bayfront beautification program

Decades of Bridge Repair

Work continued in the 60's to re-plant portions of the West Park Approach that were impacted by road widening in 1959. The circles and axial walkway system which had been part of the original design layout were obliterated and replaced with split lanes and triangular medians. By the mid-60's, the west park approach had been filled with sod and any trace of the original design had disappeared.

By the early 70's, after fifty years of wear, the Bridge of Lions was in urgent need of repair. During the next decade, much of the city's funding was directed to replacing faulty structural and mechanical parts as well as aging fixtures. The original concrete lamp posts and railing, which were beginning to crumble and topple over, were replaced with modern metal ones. Because the city focused on bridge repair, less attention was given to major improvements to the bridge's park approaches other than general landscape maintenance.



View east over bridge, 1971



View west over bridge with original lamp posts, 1967



View toward Davis Shores with lamp post replacements, 1990



Aerial view of bridge, 1960's



Aerial view of downtown St. Augustine, 1970's



1980's aerial view of west park approach just prior to landscape improvements

80's Site Improvements

While the 70's were a time for bridge repair, the 80's became a period of focusing on landscape renovation of the park approaches.

By the mid-80's a structural study was completed on the lion statues, with a recommendation to move the lions 40' back. Below is a brief account of the study's findings appearing in the St. Augustine Record June 7, 1985:

"Apparently, the concrete slabs on which the lions are mounted at the foot of the bridge have cracked and settled. Glidewell told commissioners that cracks on the lions themselves are only slight and not serious' but that the cracks on the slabs and concrete aprons are more severe. He said the statues should be moved to prevent further deterioration and the possibility that they may topple under the stress of a severe storm.

According to Rourke's report, soil erosion has occurred under the slabs and has caused the lions to tilt.

'This soil material is being lost to tidal erosion under the sloped concrete transition areas from the sea wall,' the report says. 'These concrete transition areas are also seriously deteriorated, indicating substantial cracks from settlement. Even the chain railing and concrete pedestal sections are settled, buckled, broken...

Rourke recommends that the city build new slabs about 40 feet west of the lion's current location, placing them about 10 feet into the grassy area near the bridge."

Clarissa Anderson Gibbs, the daughter of Dr. Andrew Anderson who had originally donated the lions in the mid-20's, commissioned a landscape architecture firm to re-landscape the area once the lions had been re-located. Hartwig, Willis, and Associates developed plans that included landscape areas around the lions' new locations as well as the construction of pergola on either side of the bridge's west approach. In December of 1986, Clarissa Anderson Gibbs joined the landscape architects in a dedication ceremony of the Bridge of Lions Park.

New park at bridge dedicated

By JULIA HOWARD *St. A Record*
 Staff Writer Dec 12, 1986

City officials, staff, the landscape architects and others joined benefactor Clarissa Anderson Gibbs Thursday in dedicating the Bridge of Lions Park.

Mrs. Gibbs commissioned work for the park and hired the architects, Hartwig, Willis and Associates of Jacksonville, to plan it.

"I hope it is frostproof, salt proof and sun proof," Mrs. Gibbs told the group.

The project included construction of arbors north and south of the bridge and re-landscaping the area.

City crews assisted in construction of berms, sidewalks and planters around the marble lions that guard the bridge connecting downtown St. Augustine to Anastasia Island, and they also installed a sprinkler system.

The lions for which the bridge was named were donated to the city by Mrs. Gibbs' father, Dr. Andrew Anderson, who thought the city lacked sculptural adornment.

He commissioned sculptor Romanelli of Florence, Italy, to reproduce the lions which adorned the Loggia di Lanzi in that city. Anderson died before the lions were completed, but the city accepted them from his children by resolution Feb. 22, 1936.

The resolution stated in part: "The City Commission does, on behalf of its citizens tender to Miss Clarissa Anderson and Andrew Anderson, Jr., Esq., heart-



Mayor Kenneth, Mrs. Gibbs stand at podium dedicating the Bridge of Lions Park.

felt and appreciative thanks of our people for this gift, which will be placed at, and will permanently adorn, the entrance to the new bridge and will be a perpetual memorial of its generous and distinguished citizen, whose passing is mourned."

The Bridge of Lions opened to traffic Feb. 26, 1937. The official opening and dedication of the bridge was held April 7, 1927.

In his remarks Thursday, Mayor Kenneth Beeson said Mrs. Gibbs has been, "one of the greatest benefactors in the city of St. Augustine."

He also thanked the city crew, present for the dedication, which "worked so hard and with such devotion in beautifying this loca-



Architect Bob Hartwig, Mrs. Gibbs and Mayor Beeson cut ribbon officially opening park.

Gift to aid in beauty of bridge

By JULIA HOWARD *St. A Record*
 Staff Writer May 31, 1986

The woman whose father donated the majestic lions that guard the city's downtown Bridge of Lions is continuing her father's contributions to beautifying the nation's oldest city.

Clarissa Anderson Gibbs recently retained a Jacksonville landscaping firm to design improvements to the grounds around the recently relocated lions, and the St. Augustine City Commission has approved those plans.

The plans call for construction of an arbor on each side of the west entrance to the bridge, and each arbor will shade a table and benches, said Bob Hartwig of Hartwig, Willis and Associates.

Mrs. Gibbs offered to provide landscape architectural services for overseeing the project and funding for contractors to complete the work.

The city will assist in the project by removing the damaged concrete where the lions were formerly, repair existing sidewalks and construct new ones in the plan, and providing irrigation and lighting for the park. The city will also provide the



Landscape architect Bob Hartwig shows the city commission plans for improving the west entrance to the Bridge of Lions.

benches and trash receptacles; assist with traffic control while work is under way; and transplant and reuse plants that are not part of the new plan.

Plans call for the city to complete its work prior to Sept. 1, and contractors will begin work Sept. 8. A Nov. 21 completion date has been set.

The lions had to be moved westward because the sea wall beneath them was deteriorating. They were donated to the city about 60 years ago by Dr. Andrew Anderson, who thought the city was lacking in sculptural adornment.

Landscape architect, Bob Hartwig, St. Augustine Record, May 31, 1986

Review of Impacting Documents



City of St. Augustine Parking and Transit Circulation Plan

This report, submitted to the City of St. Augustine in July of 2000, was designed to convey the concept of a transit greenway system for auto, pedestrian, and bicycle circulation within the city's historic downtown area. The city's 1999 comprehensive plan had initiated the development of this report because of its increased focus on providing safer, more efficient travel choices for both visitors and local residents within and around the city's limits.

Design Standards for Entry Corridors: Guidelines for Public Improvement

The City of St. Augustine's Historic Architectural Review Board (HARB) has taken important steps over the last decade to develop planning strategies for improving the city's historic identity. The guidelines of this document, submitted in January of 2000, focus specifically on setting standards for new development to improve the visual quality of St. Augustine's history and enhance the visitor experience.



Heritage Tourism Signage Program

The purpose of this report was to conduct a comprehensive study of existing signage, create an inventory, and develop a master plan that would create a visual hierarchy to distinguish historic, civic, cultural, and recreational destinations. The Executive Summary Report, submitted in September of 2000, focus on gateway, directional, interpretive, and regulatory signage.

Architectural Guidelines for Historic Preservation

This document, revised in November of 1997 by the City of St. Augustine's Planning and Building Division, recommends approaches for historically accurate rehabilitation (The Do's) and approaches to be avoided (The Don'ts). It also provides specifications for pre-colonial (prior to 1821) and post-colonial (after 1821) period architecture, as well as guidelines for exterior architecture of buildings, structures, objects, and sites. All relevant projects are overseen by the Historic Architectural Review Board (HARB).

Conclusion

Summary

Understanding the evolution of the Bridge of Lions' park approaches, from the time of their original construction to present day, provides a design framework for the park approach system as an integral part of the bridge rehabilitation project.

Changes to the parks' design reflect a general shift in aesthetic, recreational, and transportation requirements of local residents and visitors. As a result of the roadway expansion occurring along Avenida Menendez in the 1950's, over a quarter of the West Park Approach was lost. Bridge Rehabilitation efforts should include a plan that balances future growth and transportation/circulation needs for the downtown area of St. Augustine as well as preservation of the west and east park approaches' total area. Paying attention to the value of both of these factors in the design development process will help to maximize the downtown experience for visitors and local residents.

Next Steps

Aspects of the original design intent will be explored as a guideline for re-design of the park approaches in the rehabilitation plan. Both the original design development concepts and the actual construction plans developed in 1924 will be considered, since they represent slightly different alternatives.

One of the most important site features, the bridge's marble lion statuary, must be given priority during the rehabilitation effort in terms of how they will be moved, provided with proper storage and conservation treatment, and finally re-located. An option to consider is providing the lions with a specially designated location near the bridge during the construction process, preferably at Ponce de Leon Circle, Anderson Circle, or the Plaza de la Constitution.

Sculpture experts who are knowledgeable of the conservation treatment necessary to preserve the lion statuary's marble composition, have recommended that preventative treatments be completed on the lions annually to slow the marble's natural weathering process. Based on current research, there is evidence that little or no conservation treatment has been given to the lion statues. Educational opportunities exist for visitors and local residents, if the conservation treatment process is conducted transparently at the lions' temporary storage location, as part of a larger public exhibit about the rehabilitation effort.

All other original site elements of the west and east park approaches should be assessed for their salvage and/or replacement value. These elements include the original concrete post and chain railing detail that still stand along the bulkhead as well as thriving plant material such as the sabal palm, date palm, and India Hawthorn. Only a few of the original lanterns are still hanging for study at the Lightner Museum and the St. Augustine Historical Society Research Library. The remainder of the lanterns had been scrapped after their replacement in the 1970's, because their steel construction had been too badly rusted to salvage.

The existing conditions of all plant materials and site features will be assessed to determine which should be salvaged and re-used because of their historic value, use value, and/or overall good physical condition.

This section, Section 1, of the entire rehabilitation effort, should be used as a reference during the conceptual development phases. This data gathering and site analysis phase not only defines the historical significance of the bridge's urban context, but it serves as justification for their major role in the larger scheme of Bridge of Lions improvements.