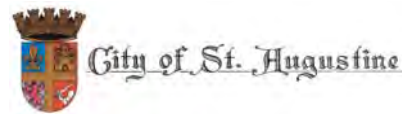
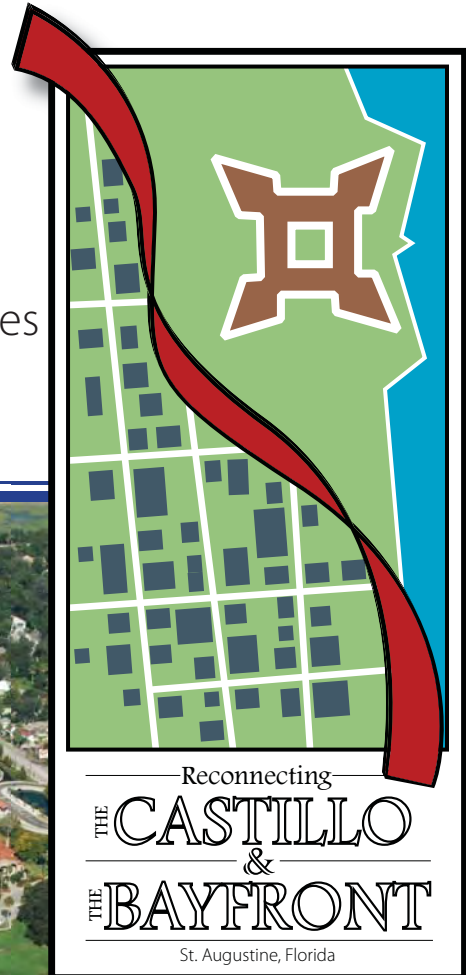
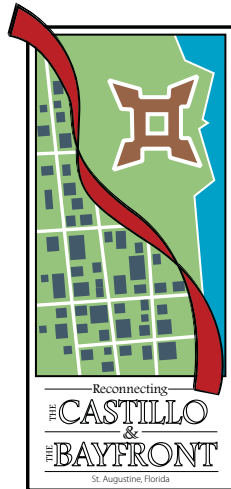


Executive Summary

Historic Downtown Streetscape Guidelines
Final Master Planning Phase | *revised September 16, 2013*



Creating Compelling Environments to Live, Work, and Play



Historic Downtown Streetscape Guidelines

The purpose of this report is to provide streetscape design guidelines for the pedestrian-focused connections in the historic district of St. Augustine surrounding and connecting to the Castillo de San Marcos National Monument. This study is funded through the Federal Transit Administration’s “Paul S. Sarbanes Transit in Parks” grant program. A synopsis of the program is below:

The Paul S. Sarbanes Transit in Parks Program was established to address the challenge of increasing vehicle congestion in and around our national parks and other federal lands. America’s national parks, wildlife refuges, and national forests were created to protect unique environmental and cultural treasures, but are now facing traffic, pollution and crowding that diminishes the visitor experience and threatens the environment.

To address these concerns, this program provides funding for alternative transportation systems, such as shuttle buses, rail connections and even bicycle trails. The program seeks to conserve natural, historical, and cultural resources; reduce congestion and pollution; improve visitor mobility and accessibility; enhance visitor experience; and ensure access to all, including persons with disabilities. The program is administered by the U.S. Department of Transportation, together with the Department of the Interior and the U.S. Forest Service.



This booklet is designed to provide a summary of the guidelines accepted by the Historic Architectural Review Board (HARB) at the August 15, 2013 meeting. HARB issued a unanimous (5-0) Opinion of Appropriateness, which requires an additional review of specific construction documents if and when projects move forward. A Certificate of Appropriateness would then be issued.

1.0 Introduction

1.1 Study Area

For the purpose of this overview, the study area is **focused on supporting increased pedestrian connections throughout the historic downtown core, especially in relationship to the Castillo de San Marcos National Monument.** Generally, the study area for the streetscape guidelines is defined by Orange Street on the north, Avenida Menendez on the east, Cathedral Place on the south, and Cordova Street on the west.

Specifically, the streets considered in the streetscape guidelines include:

Pedestrian-Only Street

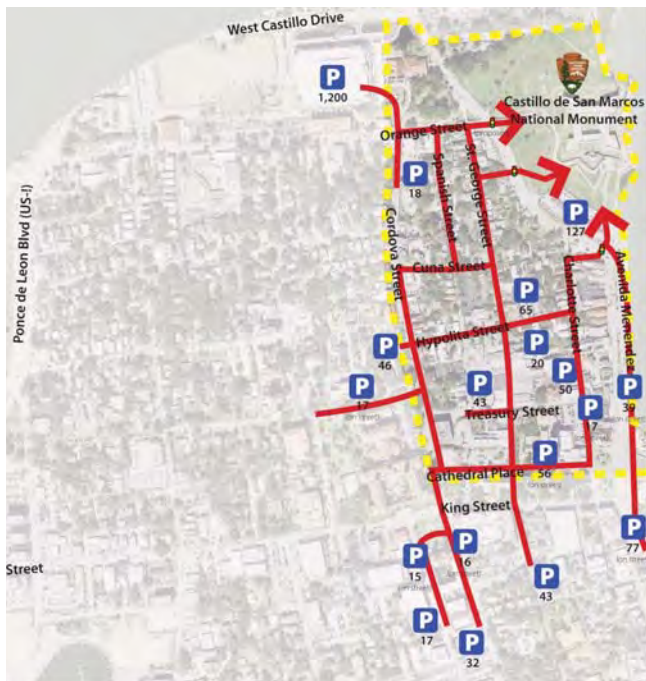
St. George Street

North-South Connectors**

Spanish Street
Cordova Street

East-West Connectors

Cuna Street
Hypolita Street
Treasury Street



**Due to the recent improvements on Charlotte Street, this was not included in the study.

1.2 Streetscape Overview

This study provides recommendations for the “visible” streetscape, including the cartway, sidewalks, street lighting, street furniture, and landscaping.

Due to the varied nature of streets in the historic district, many streets display a “typical” condition and a “narrow” condition. The narrow condition is important to consider, as it deals with the needs to **balance vehicular traffic with pedestrian flow, ADA accessibility, and obstructions such as street lights, trash receptacles, and bike racks.**

For applicable streets, both options are shown.



FIGURE A | St. George Street, ca. 1908.
Photograph from Samuel A. Cooley (on web at Shorty)

Underground utilities, while important, are not fully addressed in these guidelines. The streets within the study area largely benefit from buried electrical service (Cordova is an exception), but utilities such as water, sanitary sewer, and stormwater need to be addressed in the final design. Natural gas lines should also be incorporated. However, stormwater inlets are addressed due to their integral nature to the street and its drainage.

2.0 Streetscape Materials

2.1 Materials Overview

The streetscape guidelines presented in this document address the following items:

Curbless Street

A central component of the guidelines is the elimination of the vertical curb, instead replaced with a 12” flush curb. This achieves three main goals: (1) providing ample maneuvering room for emergency vehicles, (2) reflecting the historic “curbless” character of St. Augustine (see figure A), and (3) providing a larger pedestrian area, even in the narrowest roadways, without overly restricting the vehicular traffic.

Cartway Width

Travel lanes in the historic district range from approximately 8’ to 10’. **A fairly narrow 9’ lane is suggested**, as it balances a reasonable lane width with the pedestrian traffic that dominates downtown St. Augustine.

The 12” flush curb on either side of the roadway also helps give drivers a small clear zone on either side, and is more indicative of a typical 11’ lane found in most cities (9’ cartway + 12” curb + 12” curb).

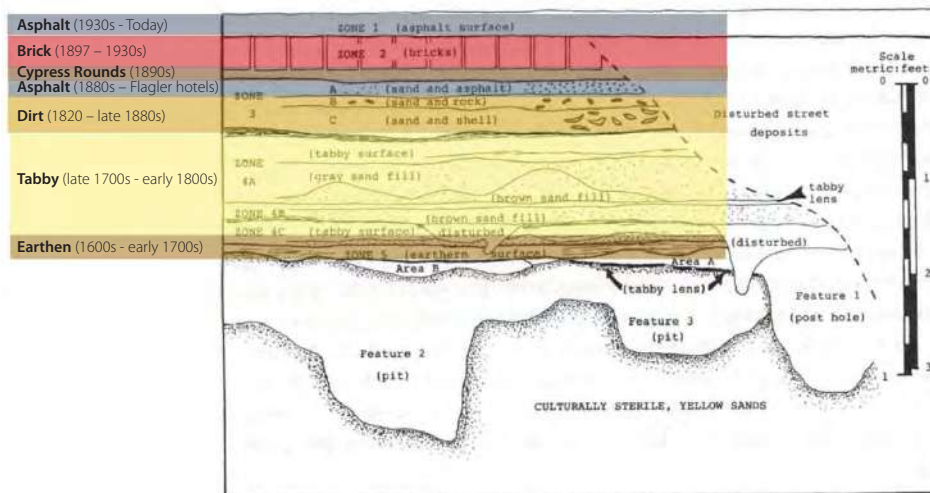


FIGURE B | Layers of St. George Street. Black and white graphic from Carl Halbirt, as published in *El Escribano: The St. Augustine Journal of History*. 1997. Carl D. Halbirt (pp. 70-97).

The 9' lane also allows for an ADA sidewalk (36" minimum) even in the narrowest roadway section (Treasury Street).

Cartway Material

Utilizing archaeological research (see figure B) and photographic evidence (see photos this page), the evolution of the street material was documented. Generally, the street remained shell (coquina / tabby) or dirt until the late 1880s, when the City of St. Augustine first began paving the downtown streets with cypress wood rounds. This was short-lived as the City then began installing brick, the first permanent street material installed throughout the historic district. Carl Halbirt, the City archaeologist, noted that there is evidence that all of the streets in the study area have had brick paving.

Interestingly, asphalt also has historic roots in St. Augustine. With his background in oil, Henry Flagler utilized asphalt (an oil-based product) around his hotels.

Based on extensive research and conversations with City staff and the Historic Architectural Review Board, it is proposed that **St. George Street remain coquina concrete** to connect the restored area at the North end of St. George Street with the colonial era Plaza de la Constitution. This also complements the Second Spanish Period architecture along St. George Street.

On the **other historic core streets, it is proposed that the cartway be brick** to reflect the first permanent paving material and to knit together the various architectural styles throughout the rest of the historic downtown core.

Street Light Fixture Type

A **four-sided Colonial style fixture is recommended** for the historic downtown core to complement the Second Spanish Period and other historic Colonial architecture (see Figure C). See the materials schedule for specific fixture call outs.

HARB noted that lighting should be as inconspicuous as possible, especially during the evenings. Other fixture types should be considered outside of the study area to compliment the surrounding architecture.

Dirt | St. George Street, ca. 1865.

Photograph from Samuel A. Cooley (on web at Shorty)



Asphalt | King Street, ca. 1891

Photograph from Detroit Publishing Company (on web at Shorty)

Cypress | St. George Street, ca. 1894

Photograph from Detroit Publishing Company (on web at Shorty)



Brick | St. George Street, ca. 1908

Photograph from Detroit Publishing Company (on web at LOC)



FIGURE C | Colonial Fixture along St. George Street (photo from St. Augustine Historic Society)

The color of the light is also important. Energy efficient LED fixtures are proposed, within a light output similar in color to an incandescent fixture. Specifically, **LED fixtures close to 2800-3000K should be utilized.**

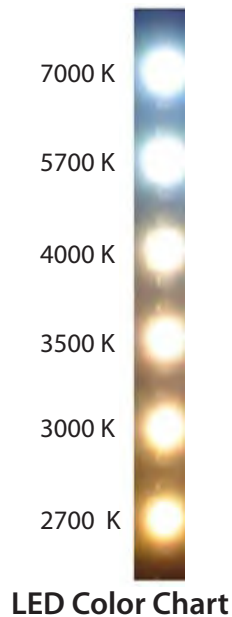
Street Lighting Placement

General placement of street lights are shown. The general height of the fixtures is suggested as 12' pole height. This results in spacing of lights approximately 36' on center.

The lights are typically shown immediately behind the flush curb, helping to define the edge of the cartway and presenting an obstacle in the vehicle path to help ensure cars stay on the cartway.

Street Furniture Placement + Selection

Typical street furniture items include benches, trash receptacles, recycling receptacles, and bike racks. Placement of the street furniture focused on maintaining ADA accessibility on the adjacent sidewalk. See the materials schedule for street furniture call outs.



HARB noted the **wooden benches** would provide more user comfort than the concrete benches currently on the street. Center armrests should be included to segment the benches into 3' sections.

Regarding trash/recycle receptacles and bike racks, HARB noted the **black or brown-black finishes** would be appropriate. See the material schedule.

Street Trees + Landscape

On St. George Street, Hypolita Street and Spanish Street, street trees are shown as an option. While a fairly recent addition to the historic core (ca. 1960s), trees do provide pedestrian comfort by adding some shade along a sometimes hot area of the city. It is suggested that **cabbage palms be used**, as photographic evidence shows these were used along Avenida Menendez, San Marco Avenue, and other important streets in St. Augustine (see figure D).



FIGURE D | Cabbage Palms along Avenida Menendez (photo from St. Augustine Historic Society) and San Marco Avenue (photo from Florida School for the Deaf and Blind).

Along St. George Street, south of Hypolita, shade trees may be appropriate due to the turn-of-the-century commercial architecture. Native trees, such as Live Oak (*Quercus virginiana*), Elm Trees (*Ulmus parvifolia*),

and Florida Maples (*Acer rubrum*) should be considered. See Figure E. HARB also encouraged private courtyards to add shade trees.

Other landscape is suggested in **planters and hanging baskets**. Note that long term maintenance from the business owners, a Downtown Improvement District, or the City needs to be considered.



FIGURE E | Shade Trees (photos by others)
LEFT: Live Oak (*Quercus virginiana*) CENTER: Elm (*Ulmus parviflora*)
RIGHT: Florida Maple (*Acer rubrum*).

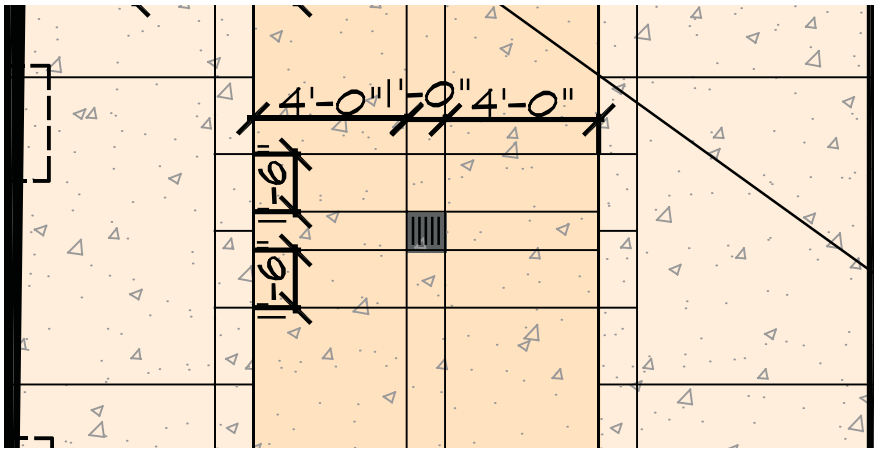


FIGURE F | Coquina Concrete street enlargement, noting placement of expansion and control joints. Note the placement of the inlet. Example shown above is south of Hypolita Street.

2.2 Enlargement: Coquina Concrete Cartway Layout

While coquina concrete has been identified as the most visually appropriate cartway material along St. George Street, the material has inherent technical challenges. Coquina concrete is prone to cracking, moreso than standard vehicular grade or even sidewalk grade concrete mixes due to the incorporation of the distinctive shell aggregate.

To overcome this technical challenge, **appropriately placed expansion joints and sawcuts are critical to direct cracking and minimize the impact to open expanses of concrete.** Recommended spacing is noted in Figure F. Additional technical recommendations are noted in the materials schedule.

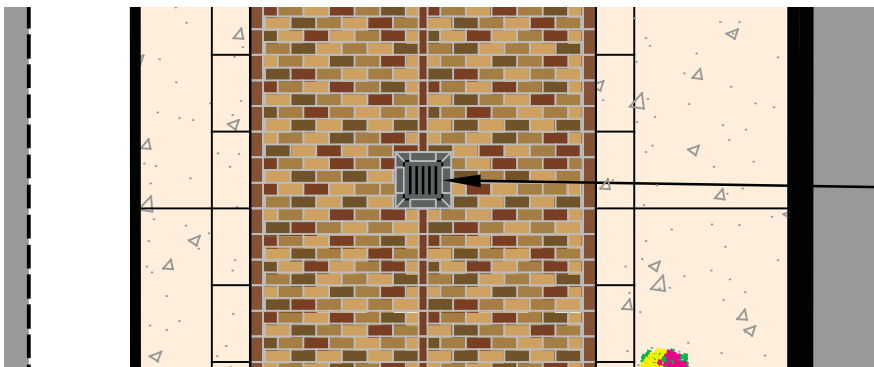


FIGURE G | Brick street enlargement, noting placement of brick along flush curb, around storm inlet, and along center of cartway. Example shown above is Spanish Street, north of Cuna Street.








2.2 Enlargement: Brick Cartway Layout

Careful detailing of the brick street not only heightens the appearance of quality and care, but it functions to better hold the “field” in place over time.

The storm inlet drains are framed with a single row of brick and the edges of the cartway also use a single row along the flush curb to compose the conclusive edge of the running bond field. The center is defined by a brick placed on its narrow side, creating a clean edge for the inverted crown roadway. Layout is noted on Figure G.







Site Furniture Matrix

The street furniture noted in the below matrix defines the design intent. Products of equal materials, dimensions, style and quality may be proposed.

Sample Image	Manufacturer	Model	Material
Benches			
	Victor Stanley	Framers Modern	Wood Slats and ductile iron castings
	Canterbury International	Bowery 500	Wood Slats and Aluminum Frame
	Thomas Steele	Waldorf	Wood Slats and Powder Coated E-Steel
Trash and Recycle Receptacles			
	Canterbury International	PADC 104-A	Cast Iron
	Landscape Forms	Scarborough	Powder-Coated Metal
	Victor Stanley	Dynasty	Powder-Coated Steel
	Thomas Steele	Gramercy	Cast Iron & Steel

Site Furniture Matrix

The street furniture noted in the below matrix defines the design intent. Products of equal materials, dimensions, style and quality may be proposed.

Sample Image	Manufacturer	Model	Material
Bicycle Racks			
	Madrax	Post & Ring Racks	Powder-Coated Steel
	Belson	Classic Bollard Bike Rack (BOL-2-SF-P)	Powder-Coated Steel
	Dero	Bike Hitch	Powder-Coated Steel
Street Lighting			
	Lumec	Square Lantern Series	Cast Aluminum Housing
	Hanover Lantern	Plymouth	Cast Aluminum Housing
	Hanover Lantern	Essex	Cast Aluminum Housing

3.0 Streetscape Materials Matrix

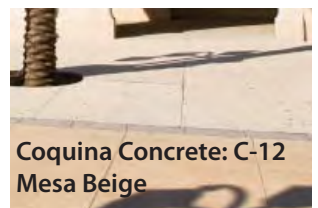
The matrix below is provided for quick reference and includes applicable technical information for planning purposes. Note that all plans will need full engineering and construction documents, which will then go through a "Certificate of Appropriateness" review with the City's Historic Architectural Review Board (HARB).

	Cartway Material	Lane Width	Curb Type & Material	Curb Width, per side	Sidewalk Material	Sidewalk Width ³	Stormwater Inlet flow & placement ⁴	Street Light Placement ⁵	Street Furniture Placement	Street Tree Type
Pedestrian Only Streets										
St. George Street	Coquina Concrete ¹ (darker color than sidewalk)	9'-0"	Flush, Coquina Concrete	12"	Coquina Concrete	6'-1" min (N) 10'-11" min (South of Hypolita)	Inverted crown, ~25' o.c. center of lane	Alternating, ~36' o.c.	Alternating, adjacent to curb	Cabbage Palms (south of Hypolita)
North-South Connectors										
Spanish Street <i>narrow</i>	Brick ²	9'-0"	Flush, Coquina Concrete	12"	Coquina Concrete	3'-0" min.	Inverted crown, ~25' o.c. center of lane	East side, ~36' o.c.	West side along outer edge, space permitting	None
Spanish Street <i>typical</i>	Brick ²	9'-0"	Flush, Coquina Concrete	12"	Coquina Concrete	4'-2" min.	Inverted crown, ~25' o.c. center of lane	Alternating, ~36' o.c.	Alternating along outer sidewalk edge	Cabbage Palm (space permitting)
Cordova Street	Asphalt	10'-0"	Raised, Granite	6" +12" gutter	Coquina Concrete (E) Concrete + Flagler Brick (W)	4'-0" min.	Crown, inlets located as existing	Alternating, ~36' o.c.	Alternating along outer sidewalk edge	Cabbage Palm (space permitting)
East-West Connectors										
Cuna Street	Brick ²	9'-0"	Flush, Coquina Concrete	12"	Coquina Concrete	5'-0" min. (may be on one side only)	Inverted crown, ~25' o.c. center of lane	Alternating, ~36' o.c.	Alternating along outer sidewalk edge	None
Hypolita Street <i>narrow</i>	Brick ²	9'-0"	Flush, Coquina Concrete	12"	Coquina Concrete	3'-9" min.	Inverted crown, ~25' o.c. center of lane	North side, ~36' o.c.	North side, along building edge	None
Hypolita Street <i>typical</i>	Brick ²	9'-0"	Flush, Coquina Concrete	12"	Coquina Concrete	3'-9" min.	Inverted crown, ~25' o.c. center of lane	Alternating, ~36' o.c.	Alternating, along building edge	Cabbage Palm (west of Charlotte)
Treasury Street <i>narrow</i>	Brick ²	9'-0"	Flush, Coquina Concrete	12"	Coquina Concrete	3'-0" min. (may be on one side only)	Inverted crown, ~25' o.c. center of lane	South side, ~36' o.c.	North side, along outer sidewalk edge	None
Treasury Street <i>typical</i>	Brick ²	9'-0"	Flush, Coquina Concrete	12"	Coquina Concrete	4'-0" min.	Inverted crown, ~25' o.c. center of lane	Alternating, ~36' o.c.	North side, along outer sidewalk edge	None

Technical Information

¹ Coquina Concrete

Recommendation for concrete shall be 6" coquina concrete with fiber reinforcement, set over 8" LBR 30 compacted subgrade. This specification should be used for sidewalks that may experience substantial vehicular traffic. Color shall be Scofield Chromix integral color or approved equal. Color is C-12 mesa beige for cartway (as shown on bottom of photo to right). Sidewalks shall be standard gray.



Coquina Concrete: C-12 Mesa Beige

² Brick

Whitacre Greer or approved equal. Mix shall be defined as: 10% Dark Antique (color 33), 20% Mulberry (color 34), 20% Mahogany (color 44), 20% Cimmerian (color 53), 30% Chocolate (color 54)



Brick

³ Sidewalk Width

Minimum 36" clear width must be maintained for ADA accessibility.

⁴ Stormwater Inlets

25' frequency of inlets is recommended to limit pooling and sloping of cartway.

⁵ Street Light Placement

Final placement of 12' poles based on photometric analysis.

Fixture Dimensions: 16"-20" wide x 36"-40" height. Color/ Material: Black Metal. LED: 2800k-3000k



Reconnecting
**THE CASTILLO
 &
 THE BAYFRONT**
 Paul S. Sarbanes Transit in Parks Grant

Streetscape Guidelines
100' Prototypes Key

Legend:

-  Project Extents
-  100' Prototype Section

Street Cartway Surface

-  Coquina Concrete
-  Brick
-  Asphalt



City of St. Augustine



Creating Compelling Environments to Live, Work, and Play



Historic Downtown Streetscape Guidelines

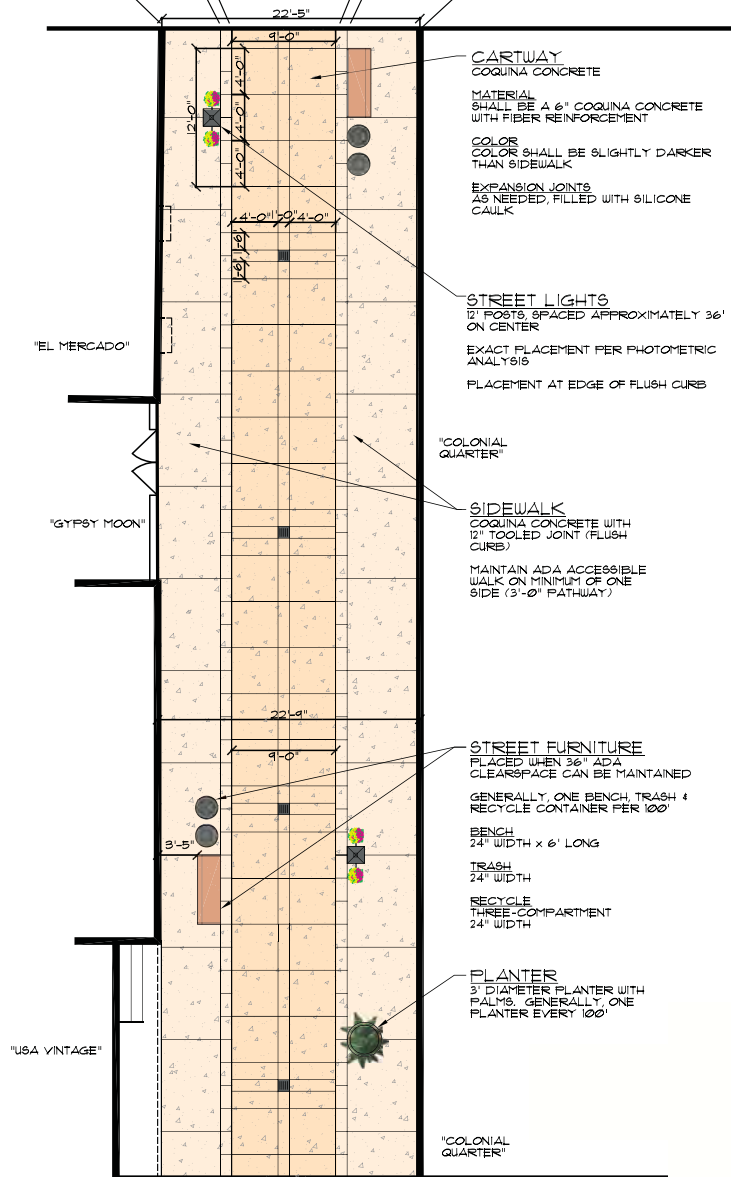
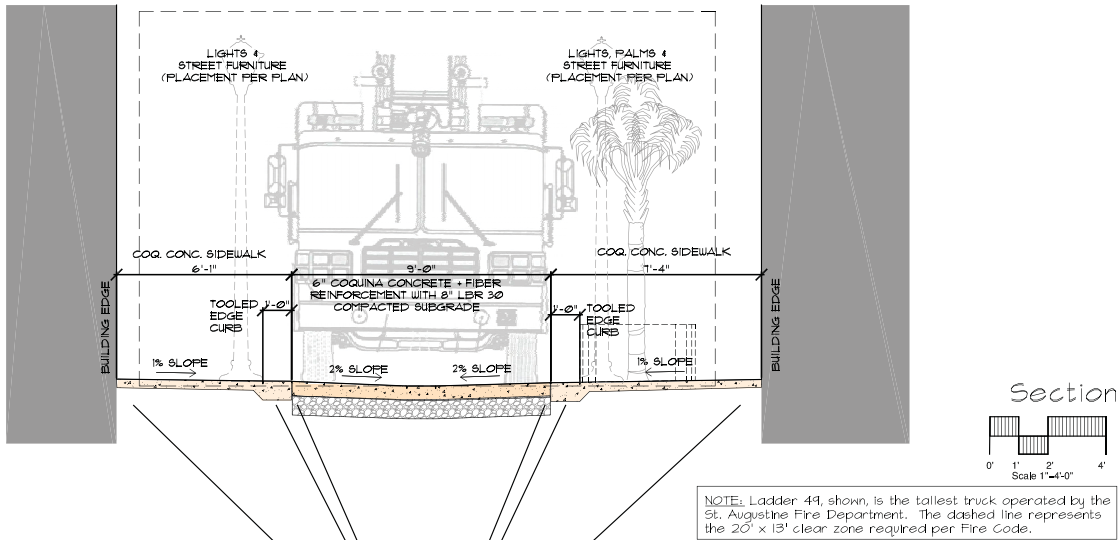
Pedestrian-Only Street

St. George Street | *north of Hypolita Street*
St. George Street | *south of Hypolita Street*

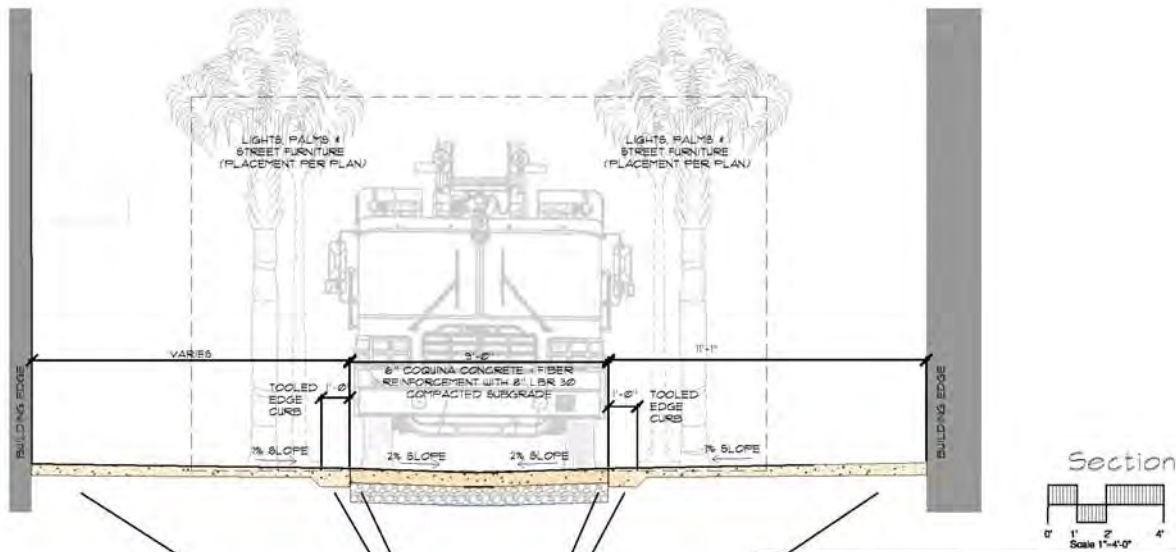


City of St. Augustine

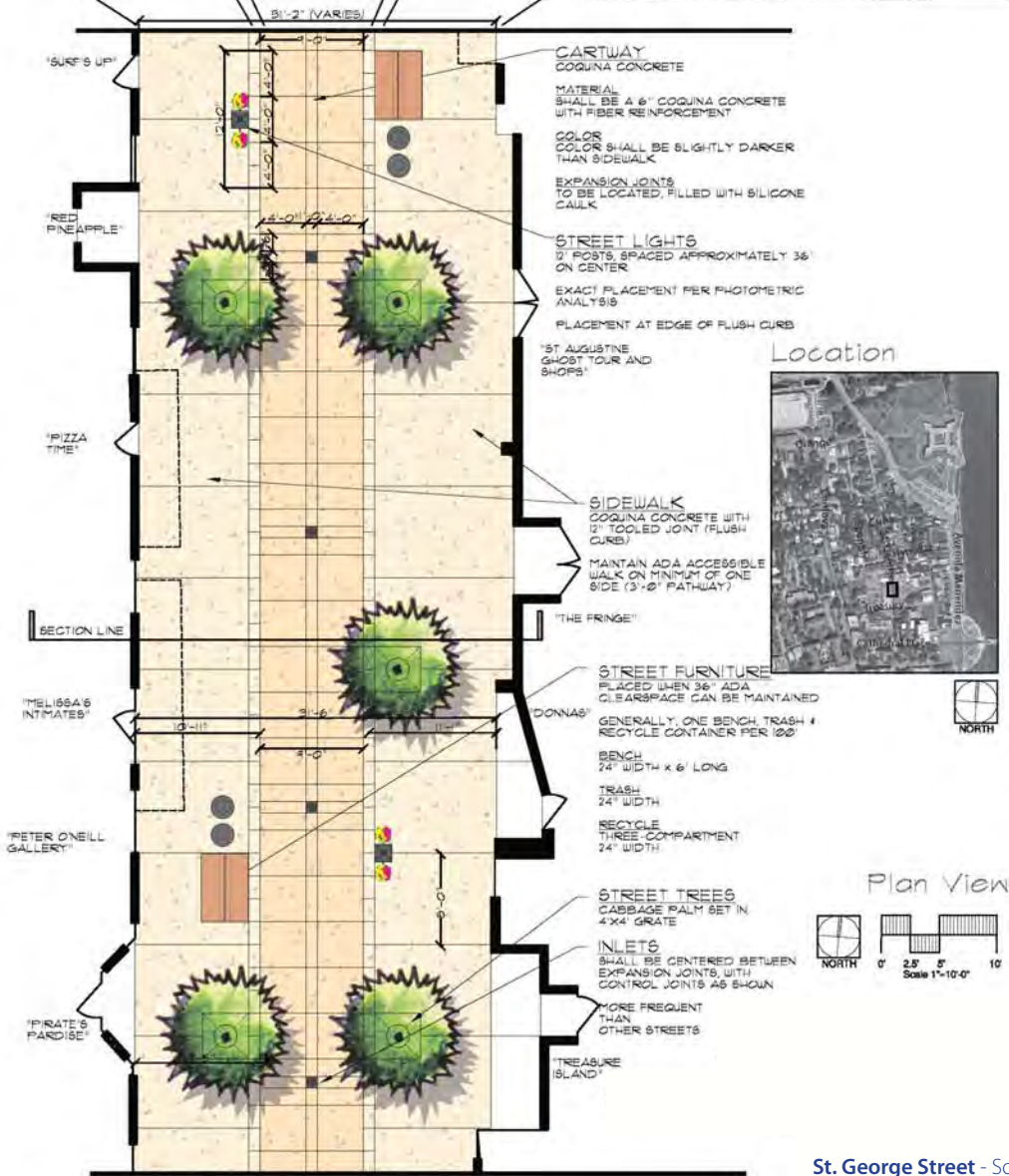




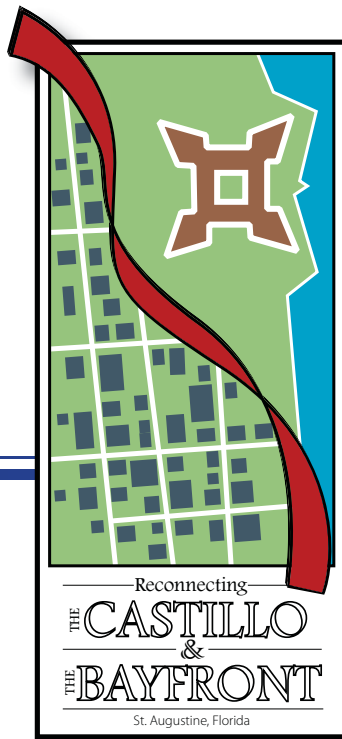
St. George Street - North of Hypolita



NOTE: Ladder 49, shown, is the tallest truck operated by the St. Augustine Fire Department. The dashed line represents the 20' x 13' clear zone required per Fire Code.



St. George Street - South of Hypolita



Historic Downtown Streetscape Guidelines

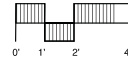
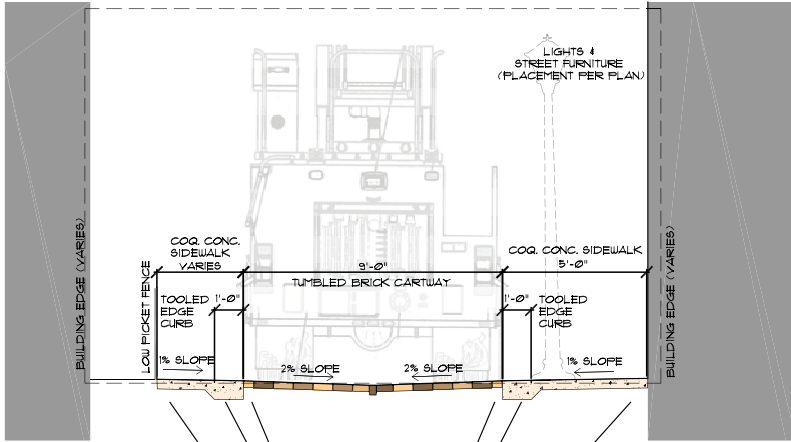
North-South Connectors

NARROW	<i>Spanish Street</i>	<i>north of Cuna Street</i>
TYPICAL	<i>Spanish Street</i>	<i>south of Cuna Street</i>
	<i>Cordova Street</i>	<i>north of Cuna Street</i>

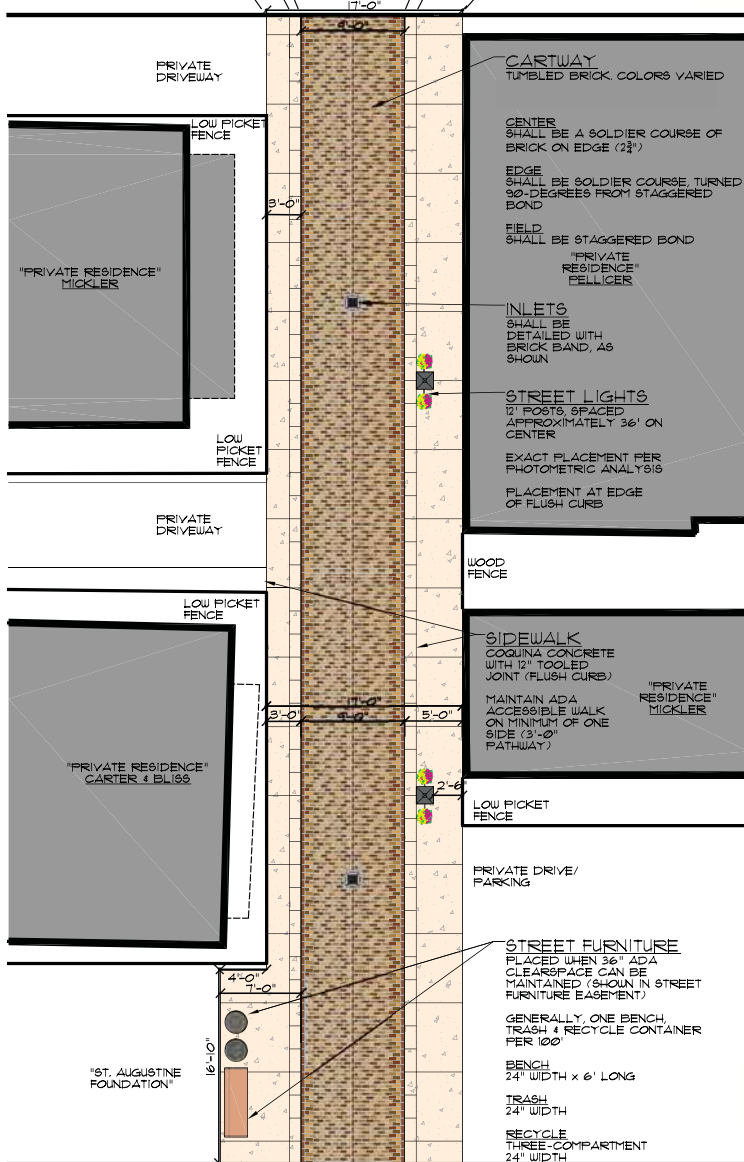


City of St. Augustine

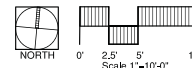




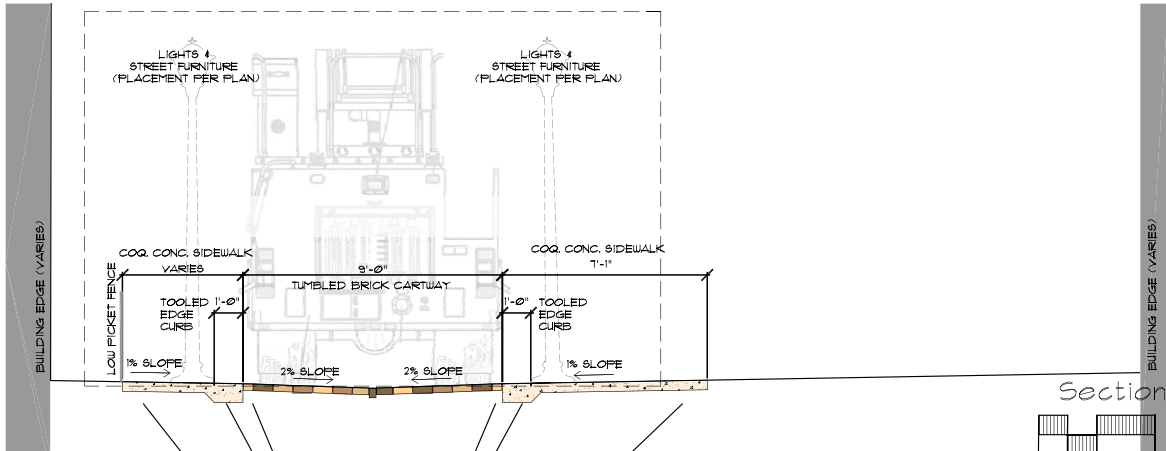
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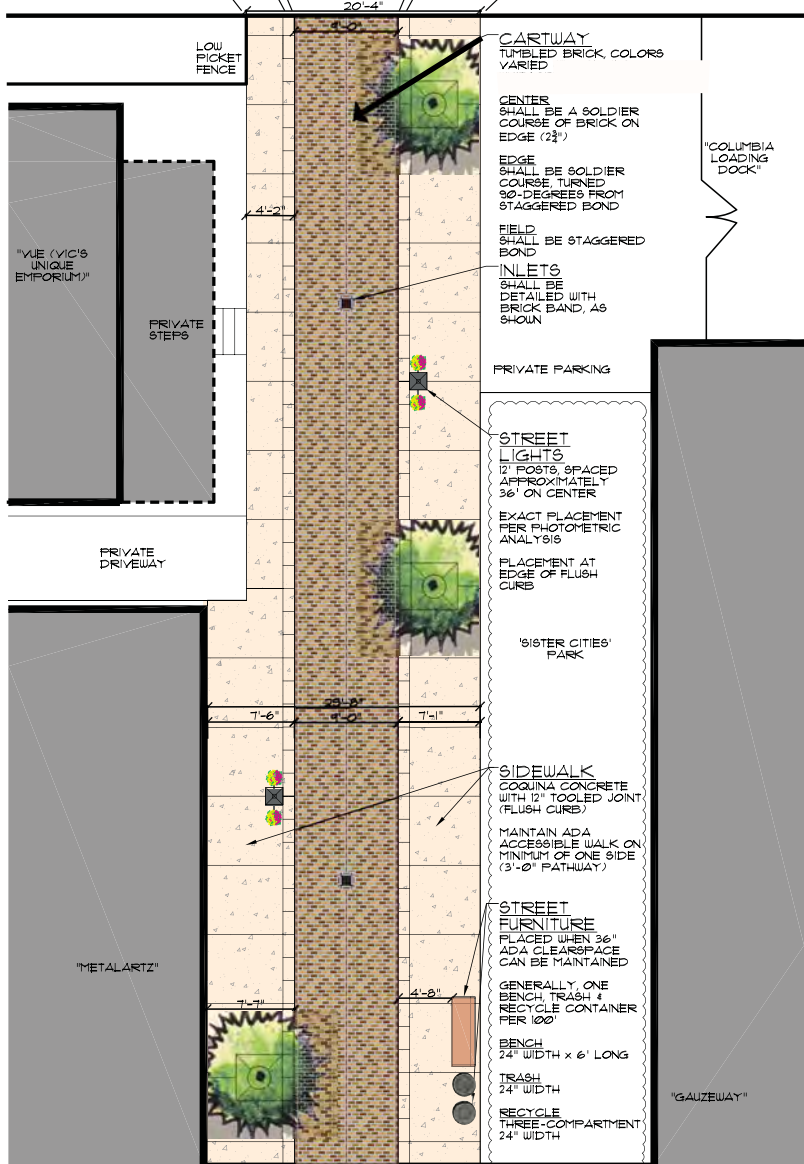
Location



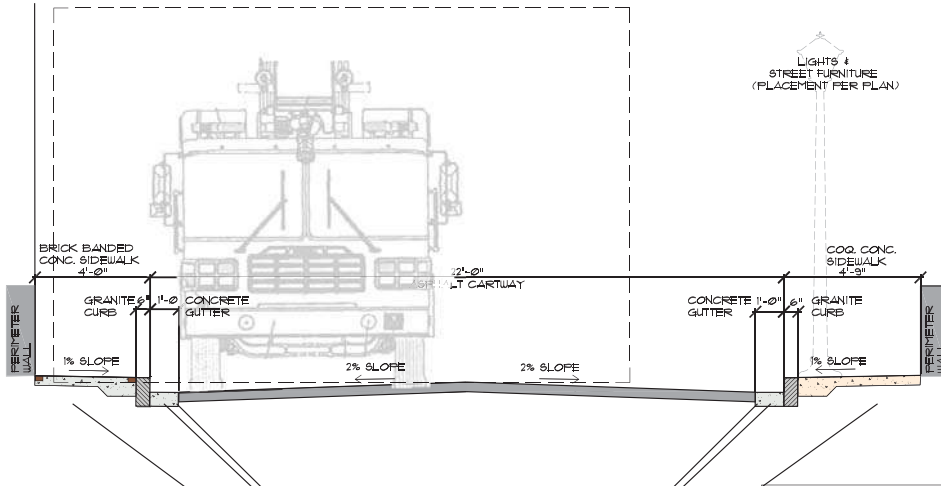
NARROW
Spanish Street - North of Cuna



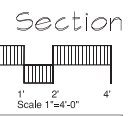
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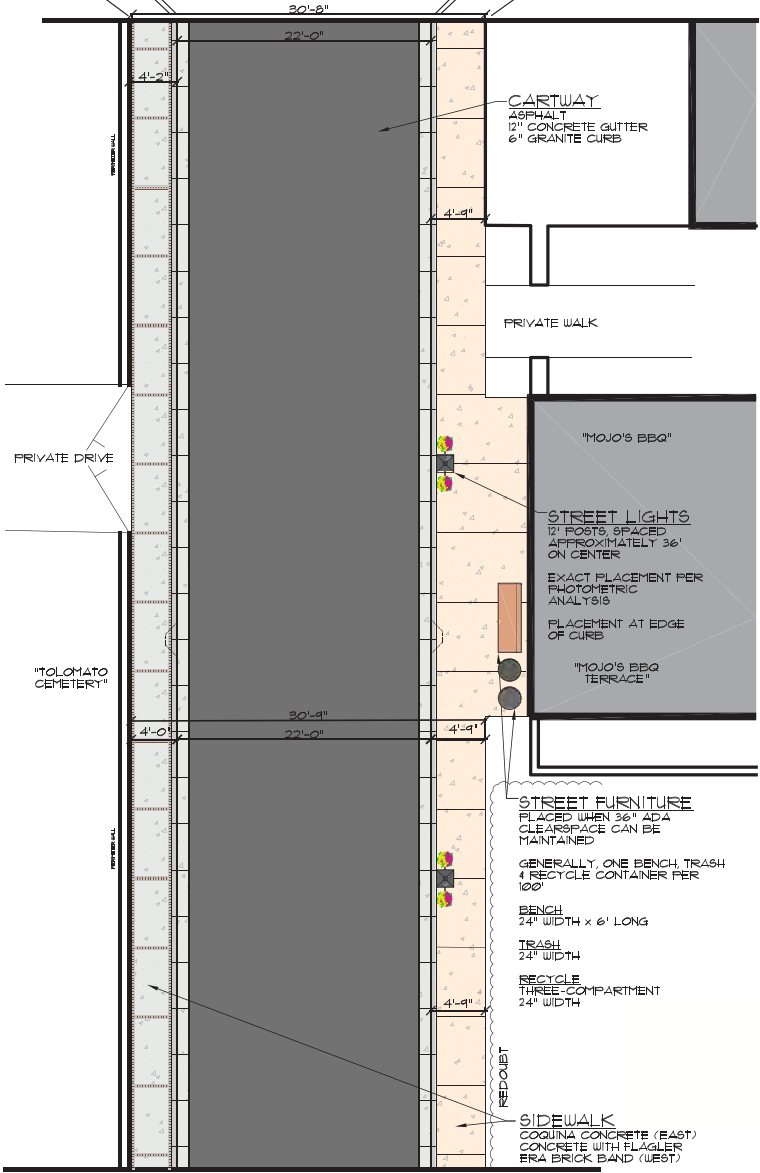
TYPICAL Spanish Street - South of Cuna



LIGHTS & STREET FURNITURE (PLACEMENT PER PLAN)



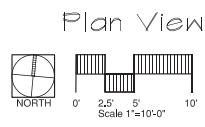
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Location

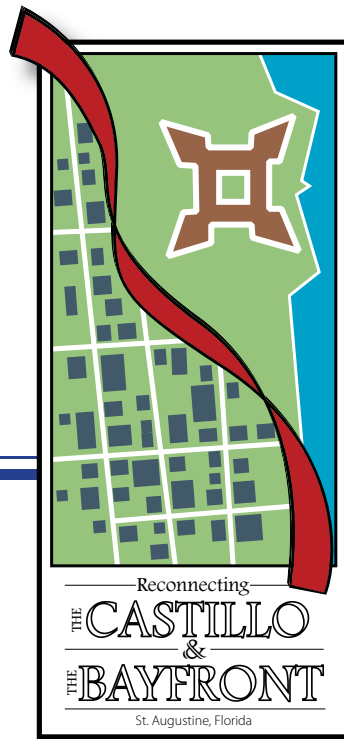


STREET FURNITURE
 PLACED WHEN 36" ADA CLEARSPACE CAN BE MAINTAINED
 GENERALLY, ONE BENCH, TRASH & RECYCLE CONTAINER PER 100'
 BENCH 24" WIDTH x 6' LONG
 TRASH 24" WIDTH
 RECYCLE THREE-COMPARTMENT 24" WIDTH



SIDEWALK
 COGUNA CONCRETE (EAST)
 CONCRETE WITH FLAGLER ERA BRICK BAND (WEST)

Cordova Street - North of Cuna



Historic Downtown Streetscape Guidelines

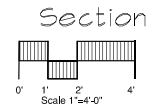
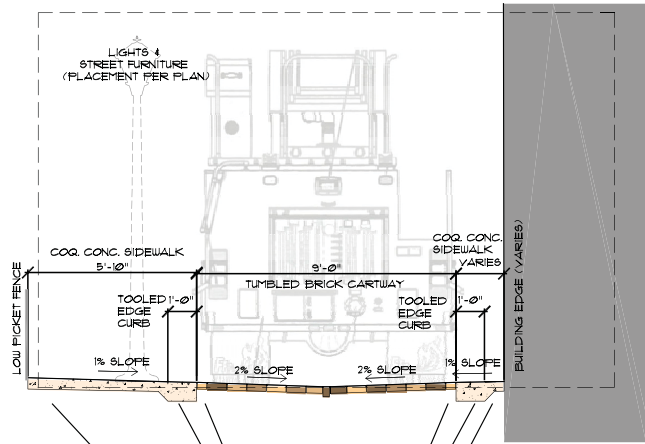
East-West Connectors

	<i>Cuna Street</i>	<i>west of St. George Street</i>
NARROW	<i>Hypolita Street</i>	<i>east of St. George Street</i>
TYPICAL	<i>Hypolita Street</i>	<i>west of St. George Street</i>
NARROW	<i>Treasury Street</i>	<i>west of St. George Street</i>
TYPICAL	<i>Treasury Street</i>	<i>east of St. George Street</i>

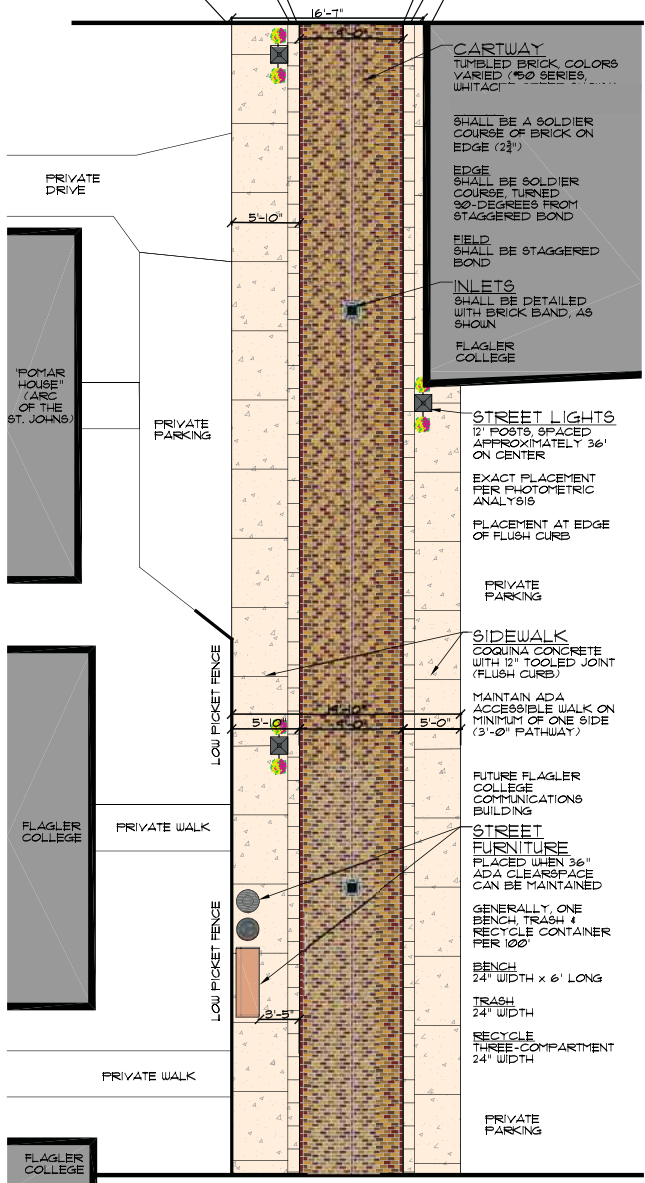


City of St. Augustine





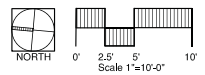
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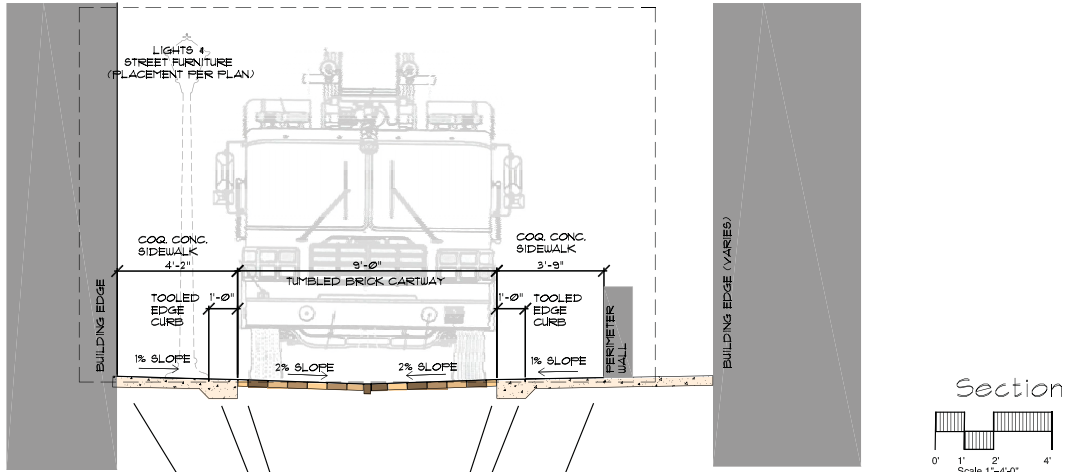
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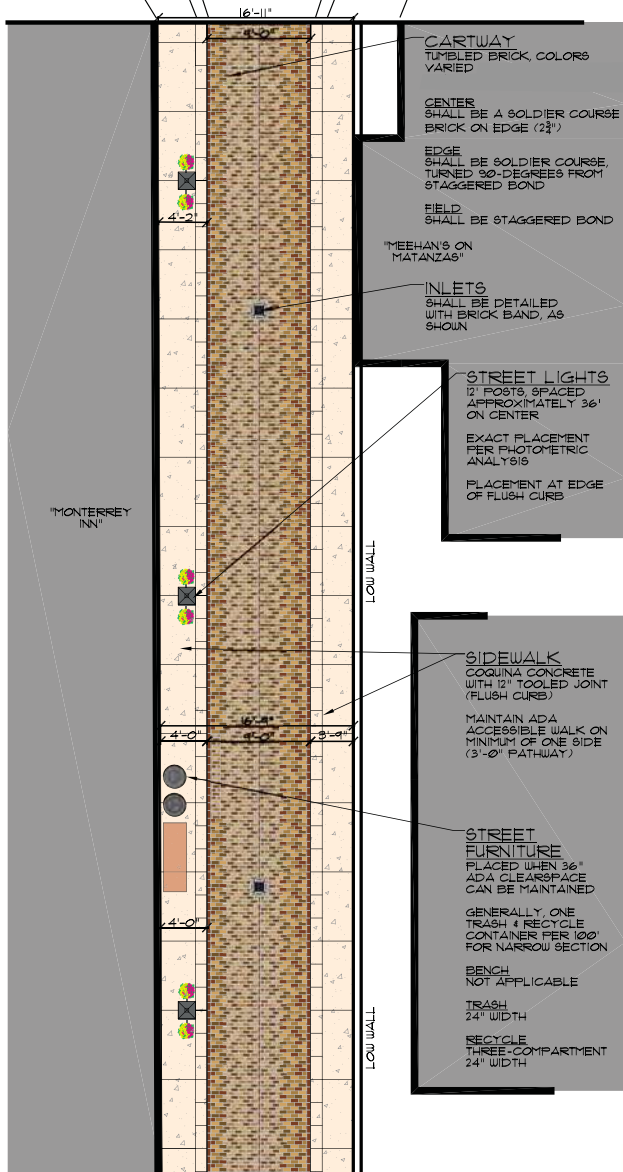
Plan View



Cuna Street - West of St. George



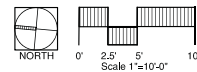
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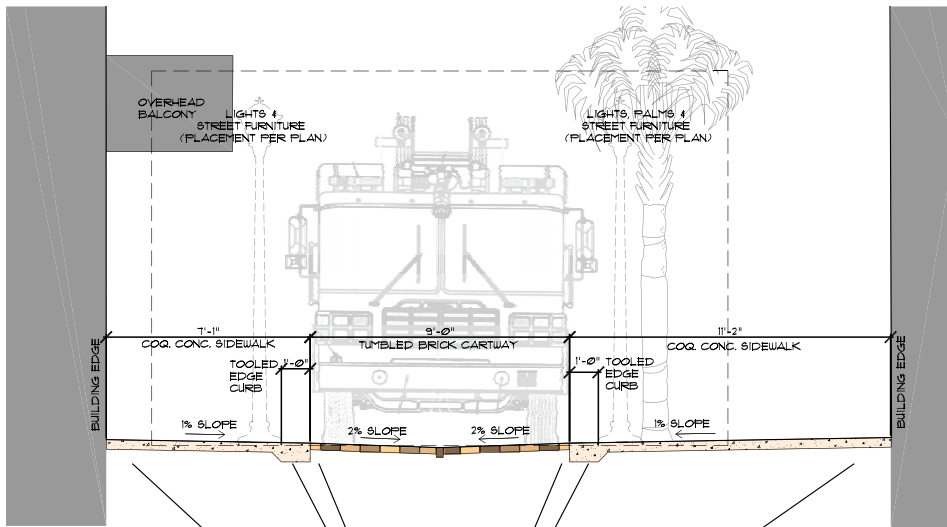
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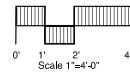
Plan View



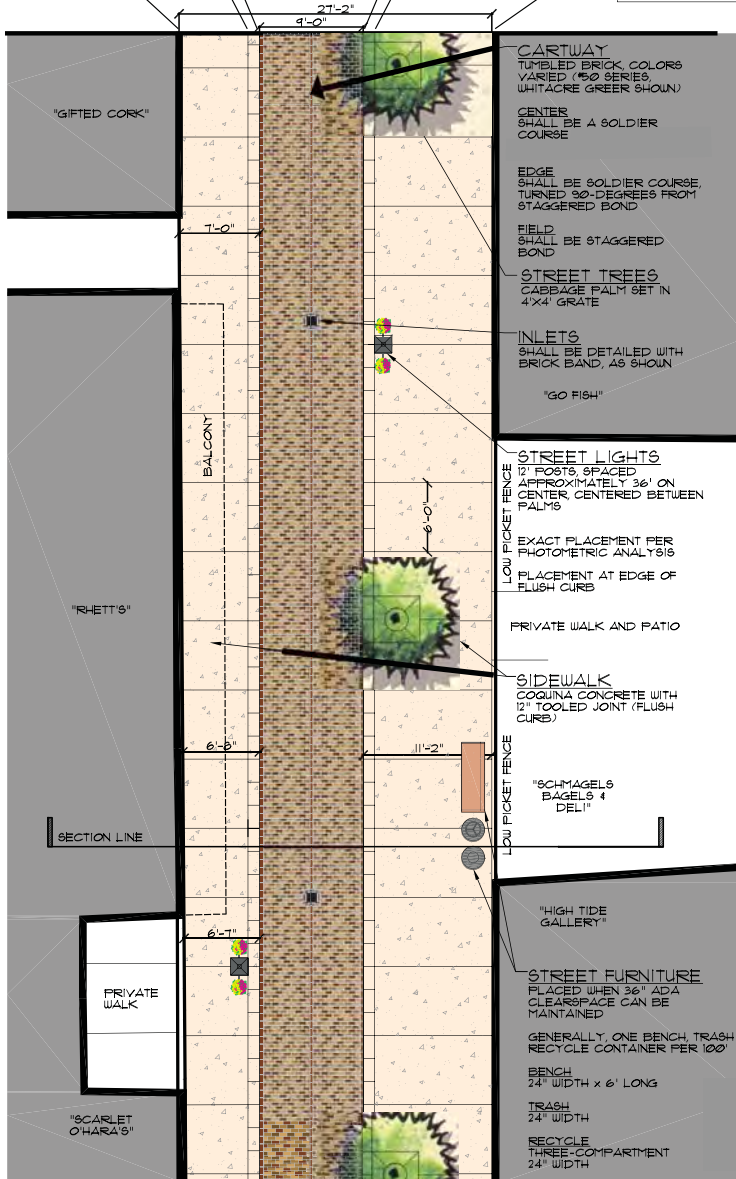
NARROW
Hypolita Street - East of St. George



Section



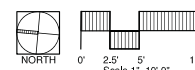
NOTE: Ladder 44, shown, is the tallest truck operated by the St. Augustine Fire Department. The dashed line represents the 20' x 13' clear zone required per Fire Code.



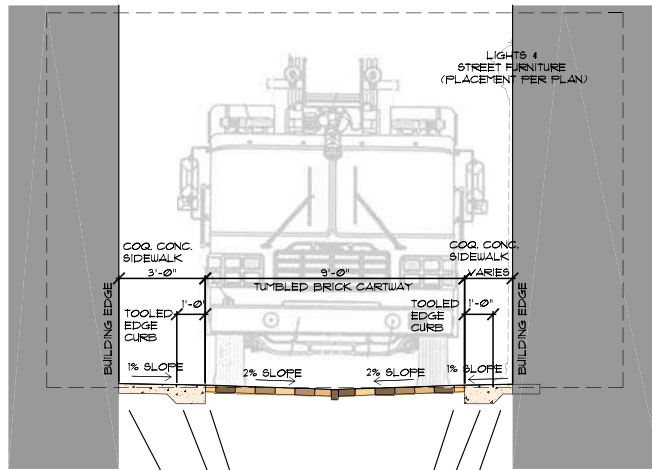
Location



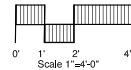
Plan View



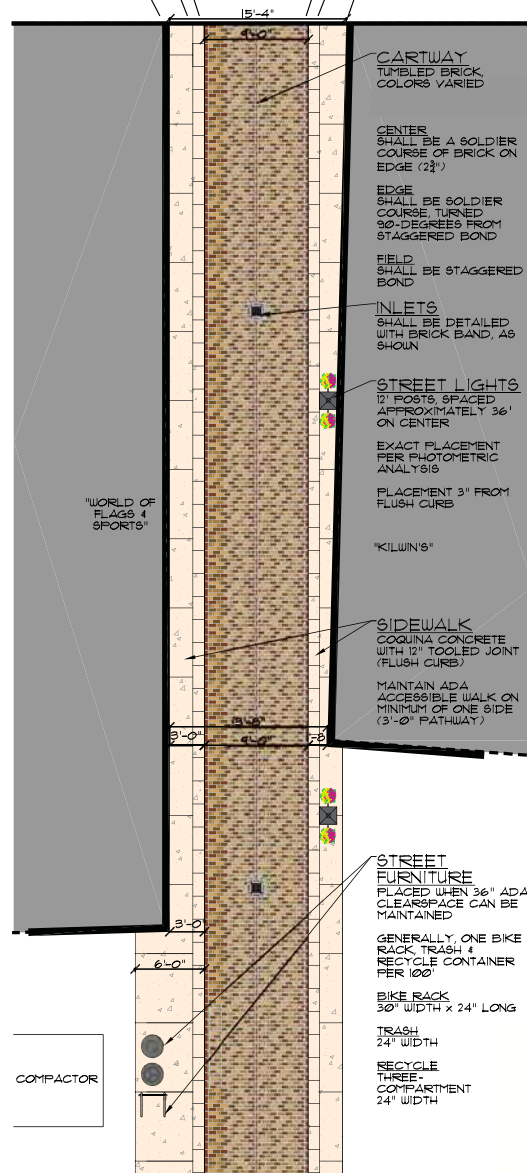
TYPICAL Hypolita Street - West of St. George



Section



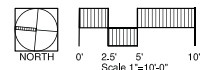
NOTE: Ladder 44, shown, is the tallest truck operated by the St. Augustine Fire Department. The dashed line represents the 20' x 13' clear zone required per Fire Code.



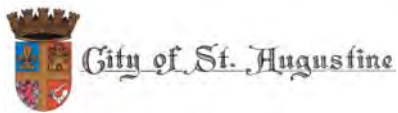
Location



Plan View



NARROW
Treasury Street - West of St. George



for more information

City of St. Augustine

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Appendix



City of St. Augustine



CITY OF ST. AUGUSTINE
Historic Architectural Review Board Special Meeting
March 21, 2013

The Historic Architectural Review Board met in formal session at 1:10 P.M., Thursday, March 21, 2013, in the Alcazar Room at City Hall, St. Augustine, Florida. Fred Halback, Chairperson, called the special meeting to order, and the following were present:

- 1. ROLL CALL:** Fred Halback, Chairperson
Len Weeks, Vice Chairperson
H. Randal Roark
Les Thomas
Paul Weaver, III

City Staff: Jenny Wolfe, Historic Preservation Planner
David Birchim, Planning Manager
Isabelle Lopez, Assistant City Attorney
Carly Mason, Recording Secretary

2. Presentation and discussion of the Transit in Parks Historic Streetscape Plan

Fred Halback recused himself, due to a conflict of interest.

Jeremy Marquis, Marquis Halback, Inc. said the discussion would include the following:

- Transit in Parks Grant Program
- Evolution of the Streetscape
- Existing Materials
- Case Studies
- Discussion of proposed materials

Transit in Parks Grant Program:

- Orange Street Implementation
- Pedestrian Connections to the Castillo de San Marcos

Evolution of the Streetscape:

- 1600 – early 1700's – Earthen
- late 1700's – early 1800's – tabby/coquina
- 1820 – late 1800's – dirt
- 1880's (Flagler) – asphalt
- 1890's – cypress rounds
- 1897 – 1930's – brick

- 1930's – Today – asphalt
- 1960's – Today – coquina concrete, brick

Mr. Marquis provided photos of the different types of streetscape material.¹

Mr. Weaver said he never thought of asphalt as a historical material and asked for additional detail regarding the use of the material. He expressed concern regarding the use of asphalt throughout the city. He preferred that brick be utilized as much as possible. He said the compromise for St. George Street was the coquina concrete and indicated that he had been present during the construction process and indicated that the street had seven layers of materials.

Mr. Marquis explained that the use of the asphalt was mainly utilized around the Flagler hotel during the 1880's and later appeared in surrounding areas around the 1930's.

Mr. Marquis said staff had completed an inventory of the city and noted that three distinct materials were used as follows:

¹ Photo's attached to packet

Existing Materials:

- Asphalt
- Coquina Concrete
- Brick

Existing Material: Cartway

- Asphalt
- Coquina Concrete
- Brick

Existing Material: Curbs

- Coquina Concrete
- Concrete
- Granite

Existing Material: Sidewalks

- Coquina Concrete
- Concrete
- Brick

Existing Material: Light Fixtures

- Gas Light
- FPL
- Colonial Fixture

Existing Material: Utilities

- Electric
- Meters
- Backflow
- Grates

Existing Material: Sign Posts

- Wooden
- Metal

Existing Material: Banners

Existing Material: Benches

- Coquina Concrete
- Wood

Material Choice:

- Faux Wood
- Ashtray
- Metal

Existing Material: Parking Meters

- Pay Stations
- Street Meters

Existing Material: Street Trees

- Wild Date Palm
- Crepe Myrtle
- Sabal Palm

Existing Material: Planters

Mr. Marquis explained that “Complete Streets” were designed for people and typically had no raised curbs and a mix of transportation uses. He provided photo examples of the following cities:²

- Copenhagen, Denmark
- Eugene, Oregon

Materials Discussion:

Cartway Surface:

(A) – Coquina Concrete

Period:

- late 1700's – early 1800's

To consider:

- Cracking/joints
- Coloring
- Staining/wear

Vehicular:

- Poor

² Photo's attached to original report

(B) – Brick

Period:

- 1897 - Today

To consider:

- Evidence of brick throughout
- Coloring
- New versus recycled brick

Vehicular:

- Good

(C) – Asphalt

Period:

- 1880's (Flagler)/1930's – Today

To consider:

- Flagler Era precedent
- Quieter/smoothen
- Long term wear

Vehicular:

- Excellent

Curbs:

(A) – Flush/No Curb

Period:

- Founding/1880's

To consider:

- Defining the Cartway
- Bollards
- ADA accessibility
- Emergency vehicles

(B) – Granite

Period:

- 1880's - ?

To consider:

- Unclear of extent utilized
- Additional texture
- Expense to install
- Painting of curbs

(C) – Concrete

Period:

- 1880's - today

To consider:

- Regular versus Coquina
- Painting of curbs

Mr. Thomas said flush/no curbs would serve as a good use for pedestrians.

Mr. Weaver said that granite should remain unpainted.

Sidewalks:

(A) – Coquina Concrete

Period:

- late 1700's – early 1800's

To consider:

- Extensive use in historic district
- Coloring
- Joints/Patterning
- ADA accessibility

(B) – Coquina Concrete

Period:

- 1880's - Today

To consider:

- Flagler Era precedent
- Coloring
- Joints/Patterning
- ADA accessibility

(C) – Brick/Brick Band

Period:

- 1897 - Today

To consider:

- Flagler Era precedent
- Full brick sidewalk versus banding
- ADA accessibility

Mr. Marquis explained that sidewalks would be difficult to install along a portion of Spanish Street.

Mr. Weeks stated that sidewalks would be a benefit for the safety of the balconies and would assist with defining vehicular traffic lanes.

Light Fixtures:

(A) – Colonial Fixture

Period:

- 1800's - Today

To consider:

- Extensive use in historic district
- Currently on St. George Street, Aviles Street, others

(B) – Gas Light

Period:

- 1900's – Today

To consider:

- Probably first “electrified”
- City standard

(C) – Eight Sided

Period:

- 1920's - Today

To consider:

- Present on Government House, Bridge of Lions
- Flagler Era precedent

Street Furniture:

- Benches
- Trash/Recycle Receptacles

Street Trees, Landscape:

- Wild Date Palms (Phoenix sylvestris)
- Cabbage Palms (Sabal palmetto)

Historic Markings:

- Street Edge
- Location marker
- Signage

Utilities:

Mr. Marquis explained that the next step would include the following:

- Develop Preliminary Layouts
- Preliminary Material Designations
- Future HARB presentations

The presentation was followed by a discussion by the board.

Mr. Roark said he would prefer that historic materials remain in there designated areas and indicated that he would not mix new materials with historic materials.

Mr. Weeks commented on the following:

- Brick used on Aviles Street appeared to work well and suggested that the material continue to be utilized
- Coquina tabby would not work well with traffic and would not be easily cleaned
- Brick streets would eliminate skateboard use and serve as a traffic calming tool
- Sidewalks would protect balconies from traffic
- Not in favor of bollards
- Would not associate asphalt within the historic areas
- Trucks that deliver within the city should be addressed to avoid further damage to structures

Mr. Marquis said sidewalks were not located on Spanish Street and indicated that Treasury Street had a partial sidewalk.

Mr. Weaver questioned whether the old bricks on Treasury Street would be recycled, and the response was affirmative.

Mr. Marquis asked for the board's feedback regarding light fixtures.

Mr. Thomas said he would prefer a four sided light.

Ron Mickler said he was in favor of the comments and emphasized the safety of the balconies and in certain areas homes which were located directly on the street and could be easily damaged. He asked that commercial trucks be further addressed at some point.

Don Crichlow said adding sidewalks to streets that don't currently have them would lose the historic feel. He said when trying to adapt to modern vehicles you lose a lot of St. Augustine's character. He indicated that sidewalks were not historically used because people would walk in the street.

John Rayno, Assistant Fire Chief, asked that the Fire Department be included in future discussions to ensure the safety of the city and residents and to be able to serve the public in a timely manner.

Mr. Thomas suggested creating a service area for trucks to eliminate traffic on Cuna, Spanish, and Treasury Streets.

3. Adjournment

There being no further business, the meeting was adjourned at 2:06 P.M.³

Len Weeks, Chairperson⁴

³ Transcribed by Carly Mason

⁴ Fred Halback's term expired and new chairperson chosen

CITY OF ST. AUGUSTINE
Historic Architectural Review Board Regular Meeting
August 15, 2013

The Historic Architectural Review Board met in formal session at 2:00 P.M., Thursday, August 15, 2013, in the Alcazar Room at City Hall, St. Augustine, Florida. Len Weeks, Chairperson, called the regular meeting to order, and the following were present:

- 1. ROLL CALL:** Len Weeks, Chairperson
Les Thomas, Vice-Chairperson
H. Randal Roark
John Valdes
Paul Weaver, III

City Staff: Jenny Wolfe, Historic Preservation Planner
David Birchim, Deputy Director
Isabelle Lopez, Assistant City Attorney
Darlene Kirkland, Recording Secretary

2. Approval of Minutes

(None)

3. General Public Comments for Items not on the Agenda

(None)

4. Certificate of Appropriateness

4. (a) F2013-0079 –Richard Joshua Parks – Applicant
Donna Parks – Owner
11 Spanish Street/PID 197480-0000¹
(Previously Tabled)

To construct a metal roof structure over an outdoor deck and patio area.

Applicant was not present.

Public hearing opened; however, there was no response.

MOTION

Mr. Thomas MOVED to TABLE Certificate of Appropriateness application F2013-0079. Motion SECONDED by Mr. Weaver.

VOTE ON MOTION:

AYES: Thomas, Weaver, Roark, Valdes, Weeks

NAYES: NONE

MOTION CARRIED UNANIMOUSLY

4. (b) F2013-0085 – John Meehan/Meehan’s Irish Pub – Applicant
Nofal-Smith, Inc. – Owner
51 Charlotte Street²
(Previously Tabled)

To remove the concrete paving material in the patio and replace with brick pavers.

Ms. Wolfe said no new information had been submitted at the time of the staff report; however, noted that the applicant had new information, since being tabled from the previous month and encouraged review of the information.

Eric Brummel, applicant, provided a drawing of the proposed project, which would

¹ Staff report attached to original minutes

² Staff report attached to original minutes

include the herringbone pattern and two different shades red flash bricks.

Mr. Weeks clarified that all existing concrete would be replaced with the bricks, and the response was affirmative.

Public hearing was open; however, there was no response.

Mr. Thomas said he was in favor of the proposed changes.

Mr. Weaver said he would be in favor of use of either color brick and noted that the brick pavers would be an appropriate choice for the area.

MOTION

Mr. Weaver MOVED to APPROVE Certificate of Appropriateness application F2013-0085 and recommended a mix of color for the brick pavers. Motion SECONDED by Mr. Valdes.

VOTE ON MOTION:

AYES: Weaver, Valdes, Roark, Thomas, Weeks

NAYES: NONE

MOTION CARRIED UNANIMOUSLY

**4. (c) F2013-0097 – BEHST Builders, Inc. – Applicant
Jesus and Marlene Piriz – Owner
264 St. George Street³**

To approve a unique color palette for the primary and secondary building and to install a bronze marker in the private sidewalk.

Ms. Wolfe said based on a review of the AGHP and without the support of evidence to the contrary, staff found that the board could APPROVE the request for a Certificate of Appropriateness at 264 St.

³ Staff report attached to original minutes

George Street for the color scheme and bronze marker with the following conditions:

1. Base and trim paint would be a flat or semi-gloss finish
2. Consideration of alternative pre-approved accent color
3. Details of the bronze marker to be provided to staff when complete

Jon Benoit stated that the owners had indicated a desire to paint the house using the colors as follows:

- Body – Lighter shade of grey with a white trim
- Shutters – Black
- Front Door – Red (recommended by staff)
- Marker would be similar to others throughout the city and would be placed inside the property line on the sidewalk

Public hearing was opened; however, there was no response.

MOTION

Mr. Thomas MOVED to APPROVE Certificate of Appropriateness application F2013-0097 to include the following staff recommendations:

- Door color – Fireweed
- Base/Trim – Flat/Semigloss
- Bronze Marker – Staff review prior to completion

Motion SECONDED by Mr. Weaver.

VOTE ON MOTION:

AYES: Thomas, Weaver, Valdes, Roark, Weeks

NAYES: NONE

MOTION CARRIED UNANIMOUSLY

4. (d) F2013-0096 – Gaere MacDonald Construction, Inc. – Applicant
George Arnold – Owner
22 Avenida Menendez⁴

To install copper roofs over the front and rear balconies.

Ms. Wolfe said based on a review of the AGHP and without the support of evidence to the contrary, staff found that the board could APPROVE a Certificate of Appropriateness at 22 Avenida Menendez for the copper balcony roofs on the west or rear elevation with the below condition and DENY the copper roof on the front balcony because it would not be compatible with the character of the historic building:

1. Documentation that the zoning standard for lot coverage could be met with the proposed balcony roof(s).

Gaere McDonald, applicant, explained that the property had ample lot coverage. He referenced A1 – primary façade of the building and noted that the roof would be taken off of the soffit, which would provide the needed height. He provided many examples of surrounding buildings that had cantilevered balconies with a roof continuation. He said an alternate roof material would be an option.

Mr. Weaver asked why Copper had been considered, and the response was consistency.

Mr. Valdes felt that tile would be more appropriate.

Mr. Weeks said he was in favor of the tile and suggested that the post be lowered.

Public hearing was opened; however, there was no response.

⁴ Staff report attached to original minutes

Mr. Thomas said he was in favor of lowering the pitch to the break of the roof and questioned the style of the post.

Mr. McDonald replied that a structural wood post would be selected and covered with Pecky Cypress.

After a brief discussion the board and applicant agreed to increase the size of the beams.

MOTION

Mr. Thomas MOVED to APPROVE Certificate of Appropriateness application F2013-0096 including the following:

- **Copper Roof (West Elevation)**
- **Tile Roof (East Elevation) and match the pitch of the break in the roof**
- **Heavier rafter material that would be close to the size of the brackets**
- **Close in the soffits and ends**

Motion SECONDED by Mr. Weaver.

VOTE ON MOTION:

AYES: Thomas, Weaver, Valdes, Roark, Weeks

NAYES: NONE

MOTION CARRIED UNANIMOUSLY

4. (e) F2013-0098 – Don Crichlow and Associates – Applicant
Robert Giebeig, Jr. et.al. – Owner
100 St. George Street⁵

To install copper gutters on portions of the north and east elevations within the courtyard areas.

Ms. Wolfe said based on a review of the AGHP and without the support of evidence to the contrary, staff found that the application meets Standards 2, 3, and 9 and

⁵ Staff report attached to original minutes

as such the board could APPROVE the request for a Certificate of Appropriateness for the gutters and site features at 100 St. George Street with the following conditions for Courtyard B:

1. Provide details on the decorative tile proposed for the wall
2. Confirm the visibility of the wall from Hypolita Street elevation
3. Provide a cut sheet for the proposed French door

Nick Trunk, applicant, provided the proposed plans to install round copper gutters. He said a wall would be built with an archway near the grille, which would customers with protection from the grilling area. He said the new wall would not be visible from Hypolita Street and would be approximately six foot off of the building. He noted that the fountain and pavers would be removed.

Mr. Weeks expressed concern with the pole lights.

Mr. Trunk replied that the pole lights were existing and had plans to install wall mounted lights and possibly removing one or both of the pole lights.

Mr. Valdes asked if the applicant would be creating an outdoor grilling area, and the response was affirmative.

Mr. Thomas asked what type of services would be offered and whether he would be taking over the surrounding stores.

John Powers replied that he planned to offer a Chipotle style grille and would be utilizing a portion of the surrounding stores.

Public hearing was opened; however, there was no response.

Mr. Weeks asked if the decking would consist of using 2 x 6 wood, and the response was affirmative.

Mr. Weaver questioned whether the proposed tile would be an appropriate material.

Ms. Wolff explained that nothing was mentioned in the guidelines that would allow tile being placed on a wall.

Mr. Trunk replied that the tiles would be a decorative accent and were not a necessity.

Mr. Trunk noted that a six foot coquina path would be built to provide a space for the grilling equipment.

MOTION

Mr. Weaver MOVED to APPROVE Certificate of Appropriateness application F2013-0098 including a stucco finish on the wall without the use of tile and the removal of the pole lights to be replaced with wall mounted lights. Motion SECONDED by Mr. Thomas.

VOTE ON MOTION:

AYES: Weaver, Thomas, Valdes, Roark, Weeks

NAYES: NONE

MOTION CARRIED UNANIMOUSLY

4. (f) F2013-0100 – Louis Saig/Harry’s Restaurant – Applicant Catalina 46, LLC – Owner 46 Avenida Menendez⁶

To construct an addition on the north elevation, expand a small building within the courtyard, and alter site features including a new courtyard wall and an opening on the Charlotte Street property wall.

Ms. Wolfe said based on a review of the AGHP and without the support of evidence to the contrary, staff found that the application required more detail to be approved as a Certificate of Appropriateness and as such staff

⁶ Staff report attached to original minutes

recommends that the application be approved as an Opinion of Appropriateness for the building and site improvements unless the applicant could provide sufficient information to the board.

Ms. Wolfe advised that additional drawing revisions were submitted which addressed the lot coverage issues.

Joe Cronk, representative, stated that the project was intended to improve access to the courtyard and service area. He said the four zones would have consistent details and provide as many options which included an addition of a covered second story, which after review would trigger a lot coverage issue. He noted the following:

- North end Patio Roof Porch – Proposed to remove low pitch metal roof and provide a roof terrace that would be accessed from the second floor
- Balcony brackets would continue in a consistent manner, which included being cantilevered out and a wrapped railing
- Roof options – Awning or open area
- Eastern bay be open to the courtyard to provide scale and shoulder room
- Existing arches would remain unchanged
- Doors and sidelight system would be relocated into the next bay

Mr. Birchim explained that the maximum size for a main structure was 2500 square foot or 50% of the lot, whichever would be less. He said the new addition on the northside of the building would require a rezoning to a Planned Unit Development because an applicant could not receive a Variance for lot coverage in the HP District.

Mr. Weaver expressed concern regarding the proposed terrace addition noting that it

would be out of proportion with the colonial style.

Mr. Thomas felt that the proposed deck would have a modern appearance

Mr. Cronk asked if the awning was approved that it be approved as a Certificate of Appropriateness because the materials would match the existing.

Mr. Thomas stated that the drawing of the proposed second story deck without a roof would not be appropriate for the Colonial style and the B1.1 sketch had a modern appearance. He questioned whether the one story addition was historic and its original use.

Ms. Wolfe replied that the historic addition had been used as a porch and had been noted on the 1924 Sanborn Map.

Exparte Communication:

Mr. Valdes said he met with Jessie Chavot and walked the site.

Mr. Roark agreed with the boards concerns.

After a lengthy discussion the board agreed that the balcony proposal would be too much for the proposal.

Entry area improvements:

- Create more defined entryway into the restaurant
- Clean up existing landscaping and provide additional waiting space
- Provide primary entry
- Provide service entry

Public hearing was open; however, there was no response.

After a brief discussion by the board the motion was as follows:

MOTION

Mr. Weaver MOVED to APPROVE Certificate of Appropriateness application F2013-0100 for items C and D and TABLE the northwest corner deck/addition and Item B. Motion SECONDED by Mr. Valdes.

VOTE ON MOTION:

AYES: Weaver, Valdes, Roark, Thomas, Weeks

NAYES: NONE

MOTION CARRIED UNANIMOUSLY⁷

5. Certificate of Demolition

**5. (a) F2013-0101 – Causseaux, Hewett, and Walpole, Inc. – Applicant
J & J Mopar, Inc. – Owner
94 South Dixie Highway**

To demolish a building that was constructed in 1952 and not located in a historic district.

Ms. Wolfe said based on a review of the information submitted by the applicant, the AGHP, applicable City ordinances, and without support of evidence to the contrary staff found that the board could APPROVE the demolition for 94 South Dixie Highway with the following conditions:

- Discuss the feasibility of relocating the building on the site immediately behind the historic pump horse trough and future marker
- Complete a historic structure form and record with the Florida Master Site File and provide a copy to staff
- Recognize that the applicant may need to consult with a Culture Resource Manager if necessary

(Inaudible)Applicant requested additional information regarding the requirements from staff.

⁷ Break from 3:57 p.m. until 4:03 p.m.

Ms. Wolfe indicated that documentation of the existing building, photos, and maps would be required prior to demolition.

The applicant noted that the structure had no historical value.

Mr. Weeks said 9 certified notices were mailed and one was returned in favor.

Public hearing was opened

B. J. Kalaidi stated that relocating the structure would be a positive change and expressed concern regarding the removal of the Oak tree, horse trough, and location of dumpsters.

Public hearing was closed.

Mr. Weeks advised that the tree concerns would be addressed by PZB and staff.

MOTION

Mr. Weaver MOVED to APPROVE Certificate of Demolition application F2013-0101 based on staff and the applicant’s testimony and to complete item 2 of the staff recommendation. Motion SECONDED by Mr. Roark.

VOTE ON MOTION:

AYES: Weaver, Roark, Valdes, Thomas, Weeks

NAYES: NONE

MOTION CARRIED UNANIMOUSLY

6. City of St. Augustine Opinion of Appropriateness

**6. (a) F2013-0090 – City of St. Augustine – Applicant/Owner
Historic Downtown Streetscape Guidelines for HP-3**

To approve the final master plan of Transit in Parks – Phase 2 for design elements including paving materials, street furniture, and landscape items.

Ex Parte Communication:

Mr. Weaver, Mr. Thomas, Mr. Valdes, and Mr. Weeks individually met with Jeremy Marquis and discussed the components of the project.

Mr. Roark said he had a phone conversation with Mr. Marquis.

Jeremy Marquis, applicant, provided a brief presentation and overview regarding the Transit and Parks Master Plan:⁸

- Asked about use of Cabbage Palms
- Brick colors and the use of existing bricks
- Proposed materials
- Street lighting
- Street furnishings
- Trash and Recycling receptacles
- Bike rack options
- Landscape and trees
- Plantings

St. George Street North:

- No street trees
- Slightly darker coquina concrete in the middle to define the cart ways
- Scouring expansion and control joints along the cart ways to eliminate the cracks
- Proposed to place storm water inlets around 25 foot on center to eliminate the rolling of water to the center of the walkway

Southside:

- Street trees – Cabbage Palms
- Cart way surface – Developed a custom brick color blend using bricks from the 30, 40, and 50 series

Spanish Street:

- Running bond brick with brick edge

- Nine foot cart way with inverted crown inlets every 25 foot
- Street lights would include bollards

Cuna Street and Treasury Street:

- Brick

Four new items to discuss:

- Street lighting
- Street furniture
- Trash receptacle/Recycle bins
- Bike racks

Mr. Marquis discussed street lighting and suggested the following:

- Four sided black Colonial fixture
- 18 inch width
- 12 foot poles to maintain dark skies
- Hurricane would be included in the design

Street Furniture: (benches)

- Black powder coated aluminum or steel
- Wood slats
- Six foot benches

Trash/Recycle receptacle:

- Black metal
- Black recycle receptacle that would be for specific uses (glass, cans, etc.)

Bicycle racks:

- Simple fixture
- Post with a ring

Landscaping:

- Larger scale plantings should be utilized in the courtyards
- Different potted plants using a variety of plants

⁸ Presentation attached to original minutes

Mr. Marquis said the priorities should be determined with the input from the community and guidance from the City Commission. He said surveys of the streets should be completed and expressed the importance of reviewing the utilities and making upgrades where necessary. He explained that the next step would include the construction and permitting process and a construction timeline would be made available to the public.

Mr. Weeks felt that the brick color recommendation was an appropriate selection.

Public hearing was opened.

B. J. Kalaidi expressed concern with cluttering the sidewalks and the maintenance repairs. She felt that the proposed bike racks would not be an appropriate style and concluded that she had concern regarding driving in the downtown area.

Wolfgang Schiu suggested that the utilities be placed underground.

Public hearing was closed.

Mr. Marquis replied that it was difficult to restrict to one design.

Mr. Thomas agreed with the street patterns, Palm trees, requiring landscaping in the courtyards, and approved of the proposed bench material.

Mr. Marquis stated that the purpose of the bike rack selection was to keep the proposal simple.

Mr. Weaver felt that the bike racks would not be too conspicuous and suggested that the styles of the receptacles be determined by the HP Districts. He said he was in favor of the brick and asked that the lighting be as inconspicuous as possible.

Mr. Marquis replied that there had been significant public input and would continue to gather input during the implementation process.

MOTION

Mr. Weaver MOVED to APPROVE Opinion of Appropriateness application F2013-0090. Motion SECONDED by Mr. Roark.

VOTE ON MOTION:

AYES: Weaver, Roark, Valdes, Thomas, Weeks

NAYES: NONE

MOTION CARRIED UNANIMOUSLY

7. City of St. Augustine Certificate of Appropriateness

7. (a) F2013-0099 – City of St. Augustine – Applicant/Owner/Lessee Bicycle Racks in HP Zones

To approve the design and installation of bicycle racks in the historic downtown area.

Ms. Wolfe explained that staff was applying to move forward with five locations for the post and ring type system, which would follow in line for the bicycle plan for St. Augustine as presented by Jeremy Marquis. She said staff followed the simple approach and would not stand out as a visible element no more than other items such as transformer boxes.

Mr. Weeks questioned the bike rack location considered for Hypolita/Spanish Street.

Ms. Wolfe explained that the bike racks would be placed near the loading zone of the Columbia Restaurant.

Public Hearing

B.J. Kalaidi asked who would be responsible for deciding the locations and

expressed concern regarding the financing of the project.

Ms. Wolfe replied that five or six locations were being considered.

Charlene Putz, Supervisor, Parks Division, explained that funding had been allocated in the current budget to start implementing the program. She said as far as the number of racks and locations information came directly from the recommendations of the 2011 Bicycle Plan.

Mr. Weeks asked how many racks would be located in each location, and the response was four or five.

Mr. Valdes questioned the number of bikes that were stolen each year, and the response was unknown.

Public hearing closed.

MOTION

Mr. Thomas MOVED to APPROVE Certificate of Appropriateness application F2013-0099 recommended the Madrax in black. Motion SECONDED by Mr. Weaver.

VOTE ON MOTION:

AYES: Thomas, Weaver, Valdes, Roark, Weeks

NAYES: NONE

MOTION CARRIED UNANIMOUSLY

8. Planning and Building Department's Report

9. Other Business

10. Review of Conflict Statements from Previous Meetings

11. Adjournment

There being no further business, the meeting was adjourned at 5:21 P.M.⁹

Len Weeks, Chairperson

⁹ Transcribed by Carly Mason